



City of Bellbrook
Downtown Streetscape Master Plan

November 12, 2021

Prepared For:



Prepared By:



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Project Purpose

The purpose of the Downtown Streetscape Master Plan is to provide a streetscape design that is consistent with and promotes the City's image and preserves the history of Bellbrook while providing for vibrant, welcoming public spaces that encourage community building that will guide both short-and long-term infrastructure and streetscape investments.

The Downtown Streetscape Master Plan is a single streetscape plan report assessing the effectiveness and physical condition of the existing streetscape, identifying new ideas and opportunities for enhancements, and prioritizing future investments based on input from City staff, project stakeholders, the general public, the City's Comprehensive Plan, and the professional knowledge and expertise of the design team.

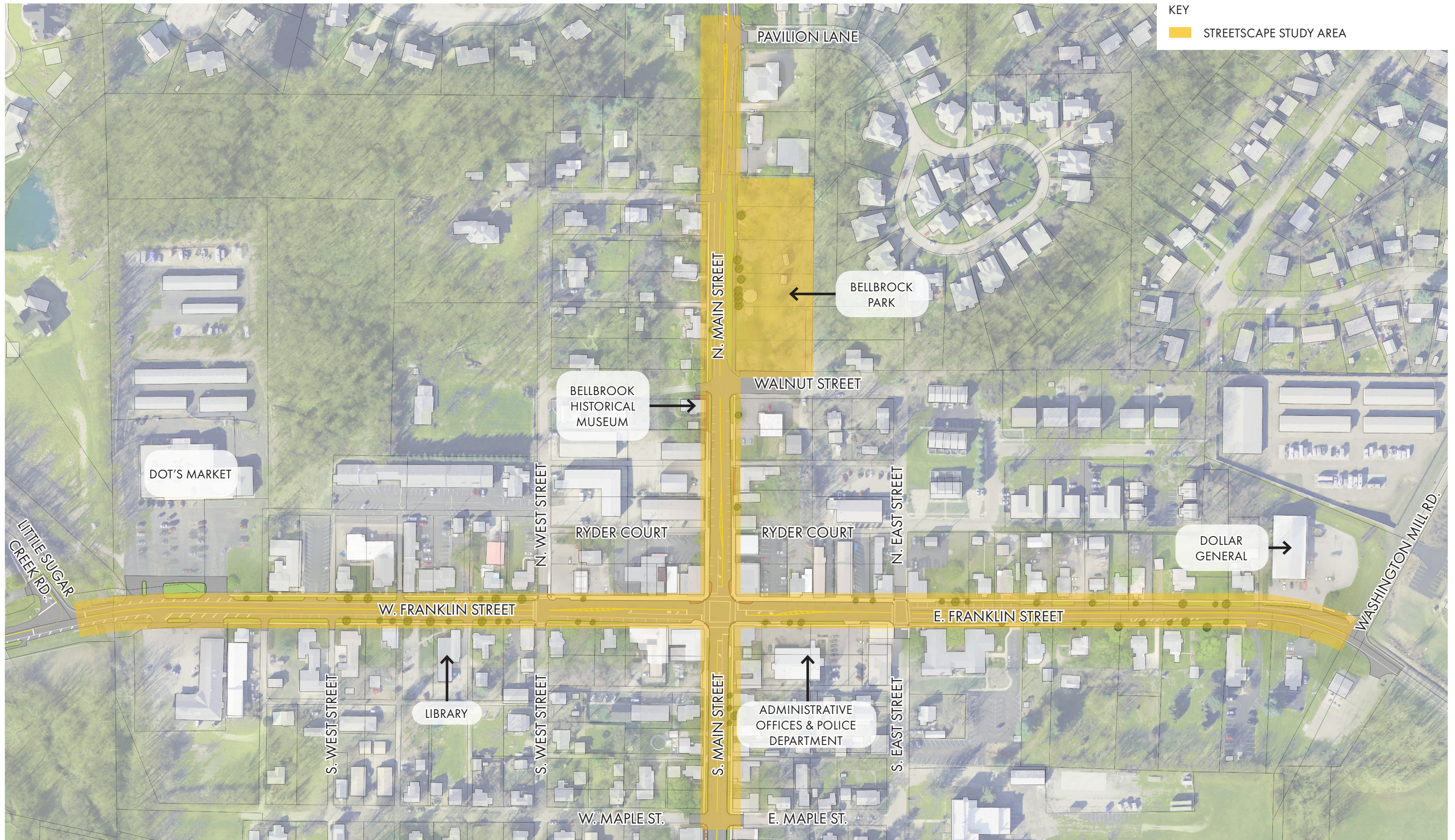
Included within the report are recommendations for location, style, and scale of streetscape amenities, materials, plantings, lighting and supporting infrastructure. The Downtown Streetscape Master Plan also includes an Opinion of Probable Construction Costs estimated for each category of improvements to be made, and Grant Funding Sources that could potentially be garnered for project implementation funding.

The Downtown Streetscape Master Plan study area includes Main Street, from Pavilion Lane to the North to Maple Street to the South, and Franklin Street from Little Sugarcreek to the West to Washington Mill Road to the East.

Project Goals

The goal of the Downtown Streetscape Master Plan is to bring new life to the downtown area and focus efforts and investment in furtherance of this goal. Therefore, the City's goals for public infrastructure in the downtown streetscape include:

- Preserve the character and history of The City of Bellbrook.
- Provide vibrant, welcoming public spaces that encourage community building and accommodate persons of all ages and demographics throughout the year.
- Enhance walkability and connectivity from residential neighborhoods while increasing visibility and effectiveness of public crossings into the Downtown District.
- Improve sidewalks with enhancements to be more inviting to pedestrians while ensuring ADA accessibility and compliance.
- Provide community branding and improved informational signage including gateways.
- Provide landscape that includes trees and native vegetation that is multi-functional, including addressing stormwater management and biodiversity concerns.
- Integrate Bellbrook Park to interface with proposed streetscape.
- Add, upgrade, and enhance amenities such as streetlights, benches, litter/recycling receptacles.
- Increase visibility and usage of existing on street parking.
- Develop a cost effective plan that can be implemented in phases.



Streetscape Study Area

↑ SCALE: 1"=200'-0"

Existing Downtown Streetscape Summary

Safety

- Along West Franklin Street, a high level of pedestrians are crossing mid-block, at Winters Bellbrook Library and the Dairy Shed, without a crosswalk.
- There is a lack of bike lane signage and markings alerting drivers to cyclists in the roadway.
- On-street parking extends into the vehicular stacking lane along Main Street at the southeast corner of the Franklin Street intersection.
- Some planters have evergreen trees that block sight distance views and are safety concerns.
- There are no pedestrian crossings across Main Street or Walnut Street to connect to Bellbrook Park.
- ADA accessible curb ramps are lacking at intersections along Franklin Street and Main Street.
- There is no pedestrian scale lighting along the streetscape.

ADA Compliance

- ADA accessible curb ramps are lacking at intersections along Franklin Street and Main Street.
- The Richard L. Frederick Bikeway at the northeast intersection of N. Main and Walnut does not have a curb ramp wide enough for a bike path.

History

- The historic monument sign in Bellbrook Park tells an interesting story, however, it is on the back of the monument in a grass area without a plaza or ADA accessibility.
- Historic buildings are marked by inconspicuous plaques.
- Historic artifacts such as a water pump and wheels are displayed along the streetscape.

Streetscape Experience

- The tree lawn has a very steep slope along N. Main Street adjacent to Bellbrook Park making pedestrians and cyclists feel exposed, unsafe, and uncomfortable while on the bikeway.
- There is a lack of sidewalk pedestrian amenity space for dining and resting areas along the streetscape.

Pedestrian Connectivity

- Crosswalks are located at most intersections and are defined by white parallel lines.
- No pedestrian crosswalks are located across Main Street or Walnut Street to provide connectivity to Bellbrook Park.
- Two curb ramps are located along Main Street at W. Walnut Street, however, they do not have ADA detectable warning pads.
- The beginning of the Richard L. Frederick Bikeway at the southeast corner of Bellbrook Park does not have a curb ramp wide enough for a bike path.
- There is no pedestrian connectivity at the intersection of E. Franklin Street and Washington Mill Road or Little Sugarcreek Road and W. Franklin Street.

Streetscape Amenities

- Bike racks have a custom design and provide City branding.
- There is a lack of biking infrastructure and bike racks located along the streetscape.
- Benches are sponsored and have a consistent traditional style.
- Litter receptacles have a consistent traditional style but do not have rain covers or allow for recycling.
- Streetscape furnishings are not located in the most strategic locations.
- Planters are provided throughout the streetscape that are planted by the Garden Club and maintained by the City.
- Some planters have evergreen trees that block sight distance views and are safety concerns.

Wayfinding and Signage

- No gateway signage welcoming travelers into the downtown streetscape is present along Franklin Street at Washington Mill Road and Little Sugarcreek Road.
- No informational signage provided throughout downtown such as a map of the downtown area and attractions.

Branding

- City branding is lacking along the streetscape.
- The streetscape is lacking gateway elements at either end of Franklin Street.

Parking

- There are 125 existing on street parking spaces.
- On street parking spaces aren't always striped creating inefficient parking along the streetscape.
- Parking signage isn't clear, creating confusion of where on-street parking is and is not permitted.
- On-street parking is limited due to the number and spacing of access points along the roadway.

Roadway

- Wide striped shoulders make the roadway feel large, increase travel speeds, and is pedestrian unfriendly.
- Wide access points into driveways are pedestrian unfriendly and in some cases unnecessary.
- Curbs vary in height and material whilst some are damaged or don't exist.
- There is a lack of bike lane signage and markings alerting drivers to cyclists in the roadway.

Street Trees

- Areas along the streetscape do not provide for consistent shade for pedestrian comfort.
- Some trees along the streetscape are overgrown, dying, or growing into power lines and need to be removed.

Existing Downtown Streetscape Summary

Drainage

- Open drainage channels between the road and the sidewalk in areas along Franklin Street create an unpleasant pedestrian experience and aesthetic.
- Stormwater gathers at the northwest corner of the Franklin Street and Main Street intersection.
- Concrete flumes located along N. Main Street are not aesthetically pleasing.

Lighting

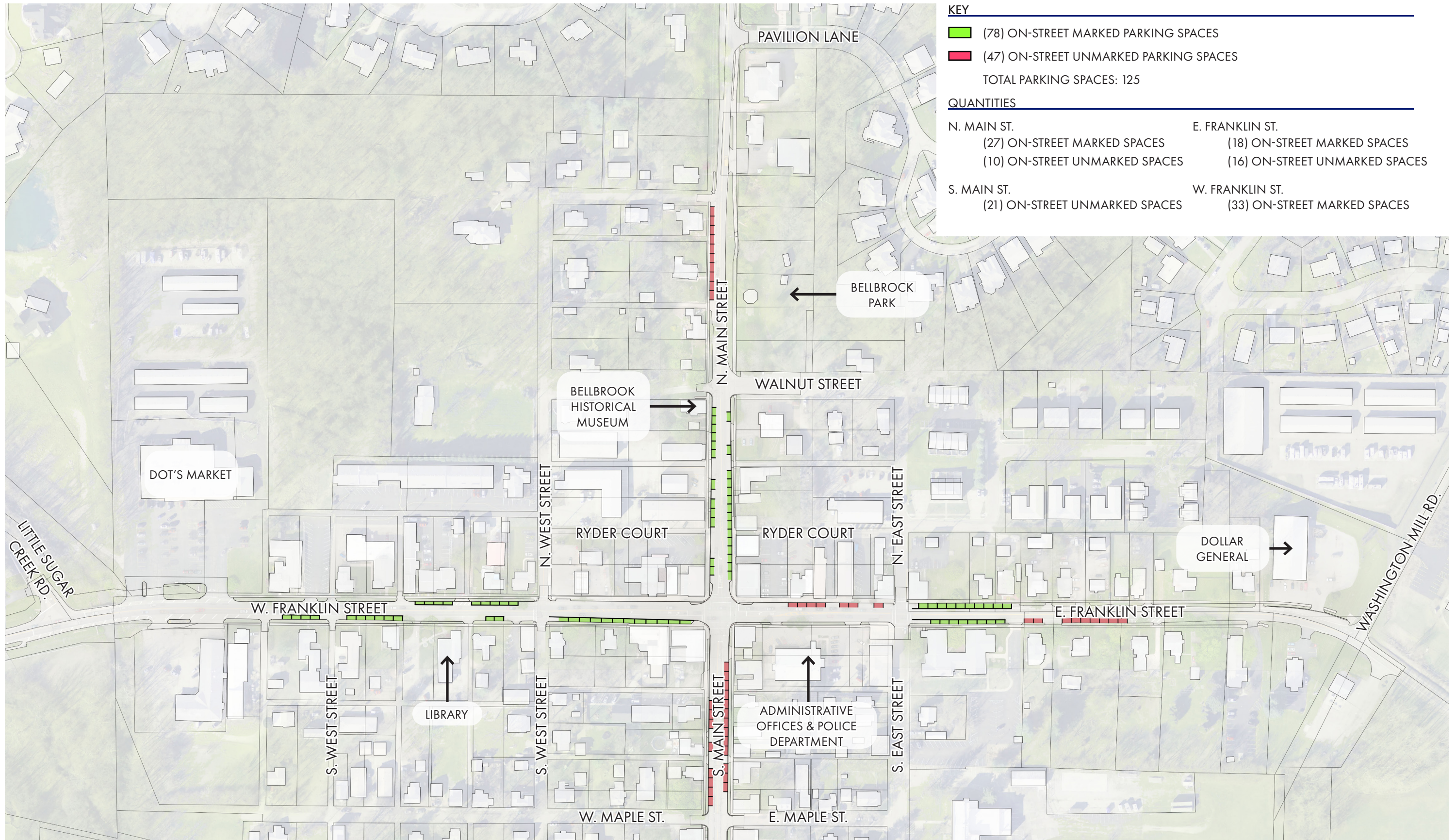
- Lighting is currently geared towards vehicular traffic and is unattractive.
- Lighting is not pedestrian scale along the streetscape, however, there is one traditional style lamp post.
- Lighting fixtures are not equipped with electric outlets for holiday décor, banners, or planters.

Utilities

- Overhead utility wires are present along the streetscape.
- Span wire for traffic signals create clutter and should be removed.

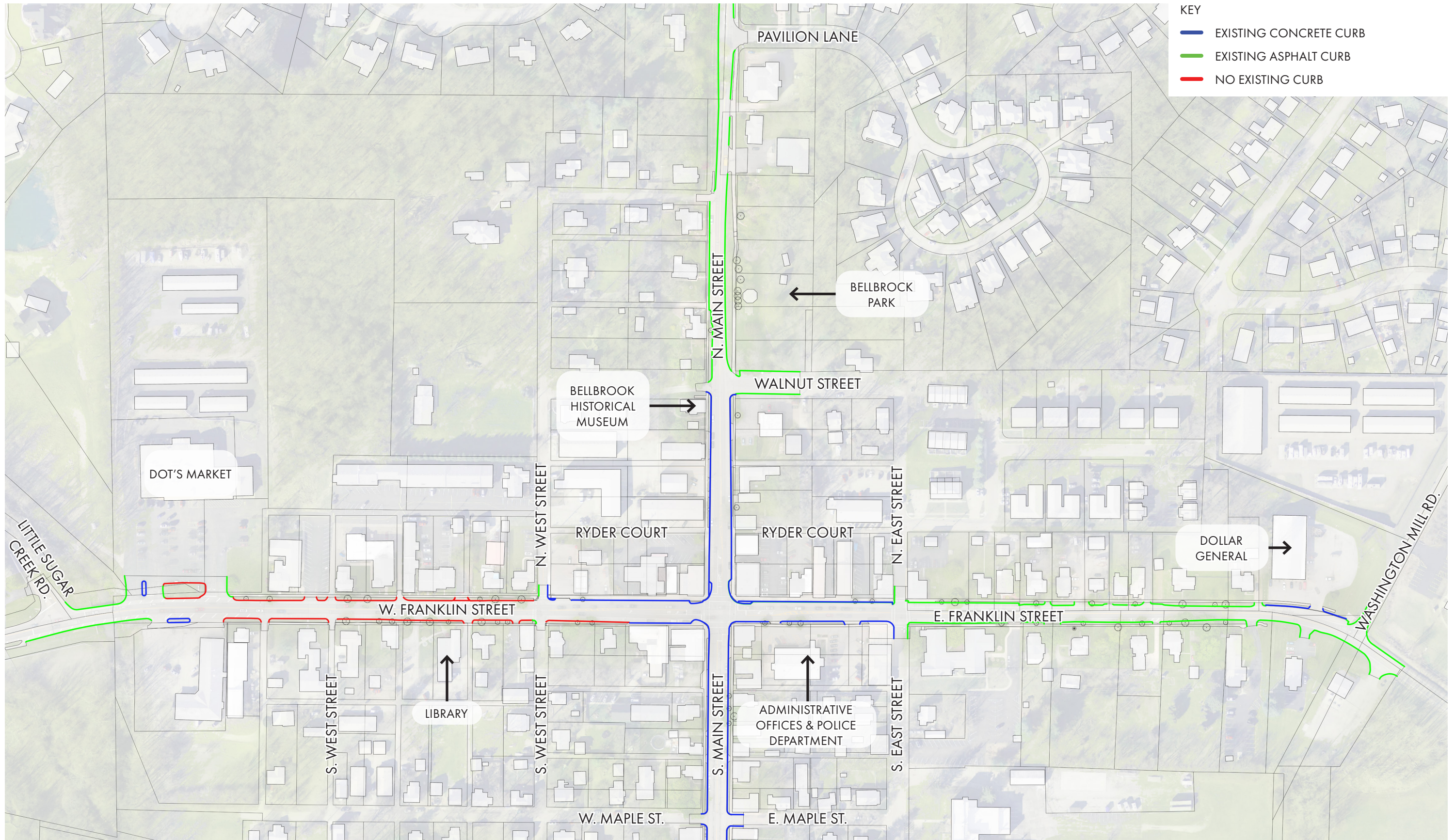
Sidewalk Pavement

- Sidewalks are narrow and vary in width throughout the streetscape.
- Concrete sidewalks are cracking, have inconsistent jointing patterns, and are uneven creating tripping Hazards.
- ADA curb ramps are not located at all intersections.
- Tree lawns between the roadway and the sidewalk reduce the amount of pedestrian amenity space along the streetscape.



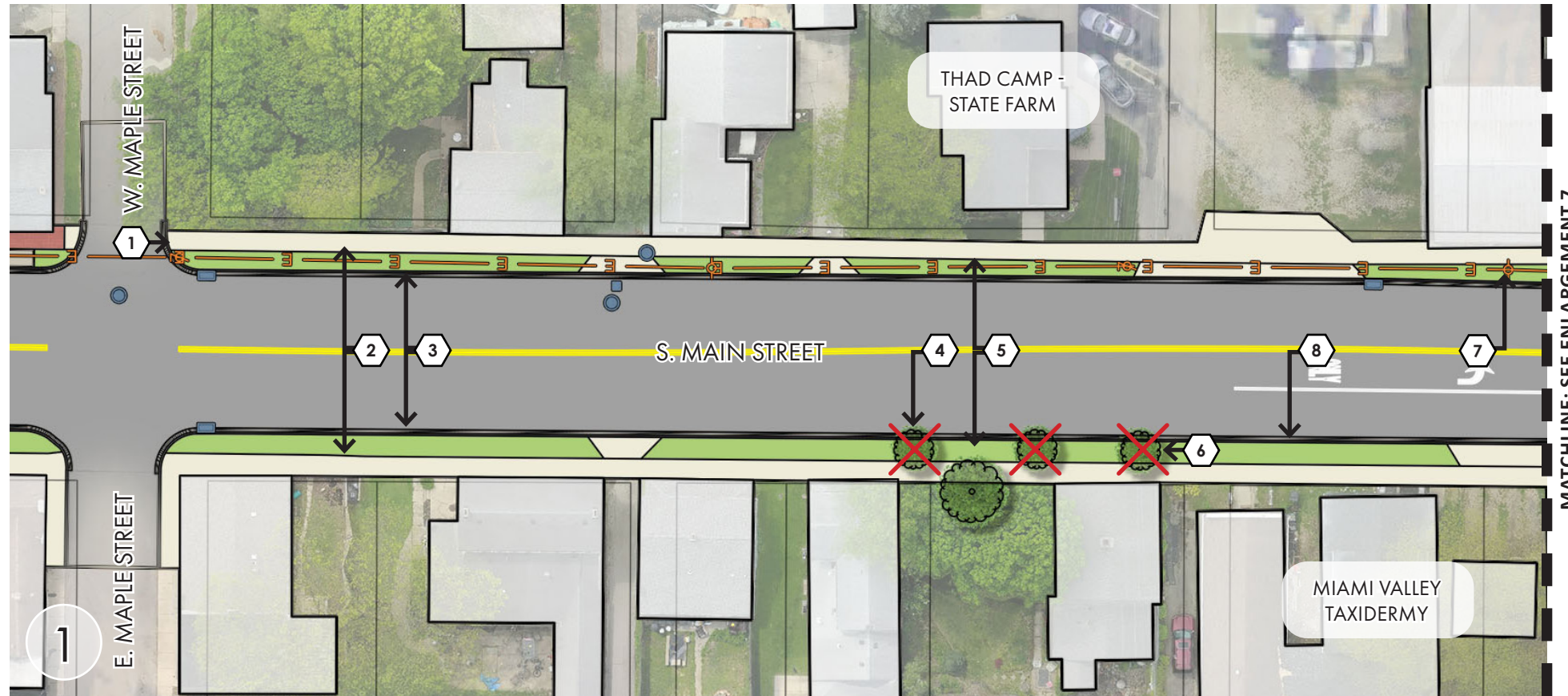
Existing Downtown Streetscape Parking

SCALE: 1"=200'-0"



Existing Downtown Streetscape Curbs

↑ SCALE: 1"=200'-0"



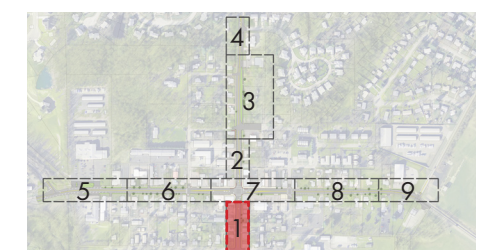
LEGEND

| | | | |
|-------------------|--------------------------|------------|---------------------------------|
| CONCRETE SIDEWALK | OVERHEAD ELECTRICAL LINE | LIGHT POLE | STREET TREE TO BE REMOVED, TYP. |
| PAVERS | UTILITY POLE | BENCH | |
| TREE LAWN | FIRE HYDRANT | BIKE RACK | |
| ADA WARNING PAD | MANHOLE | PLANTER | |
| SIGN | STORM DRAIN | TRASH CAN | |

PROPOSED KEY NOTES

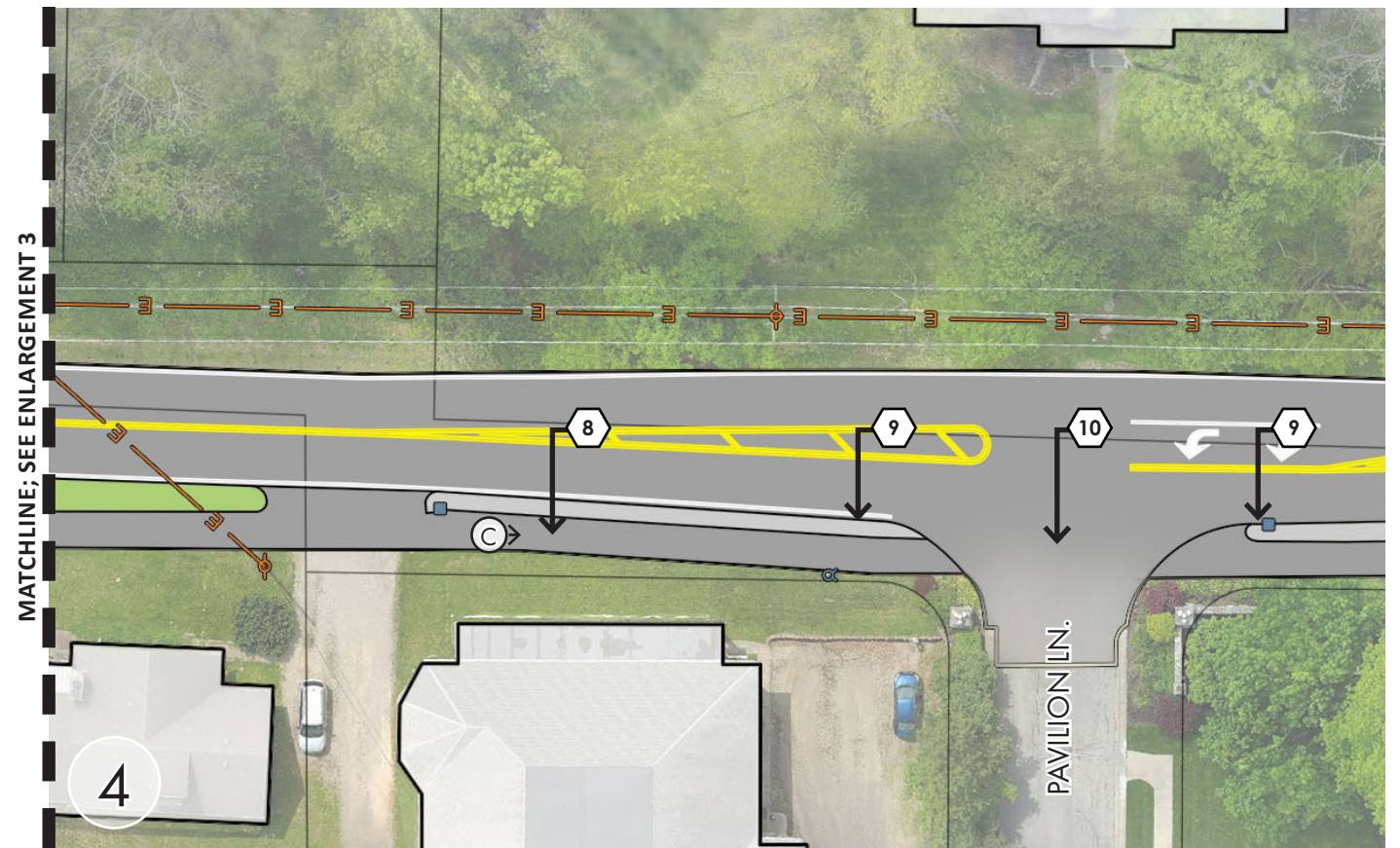
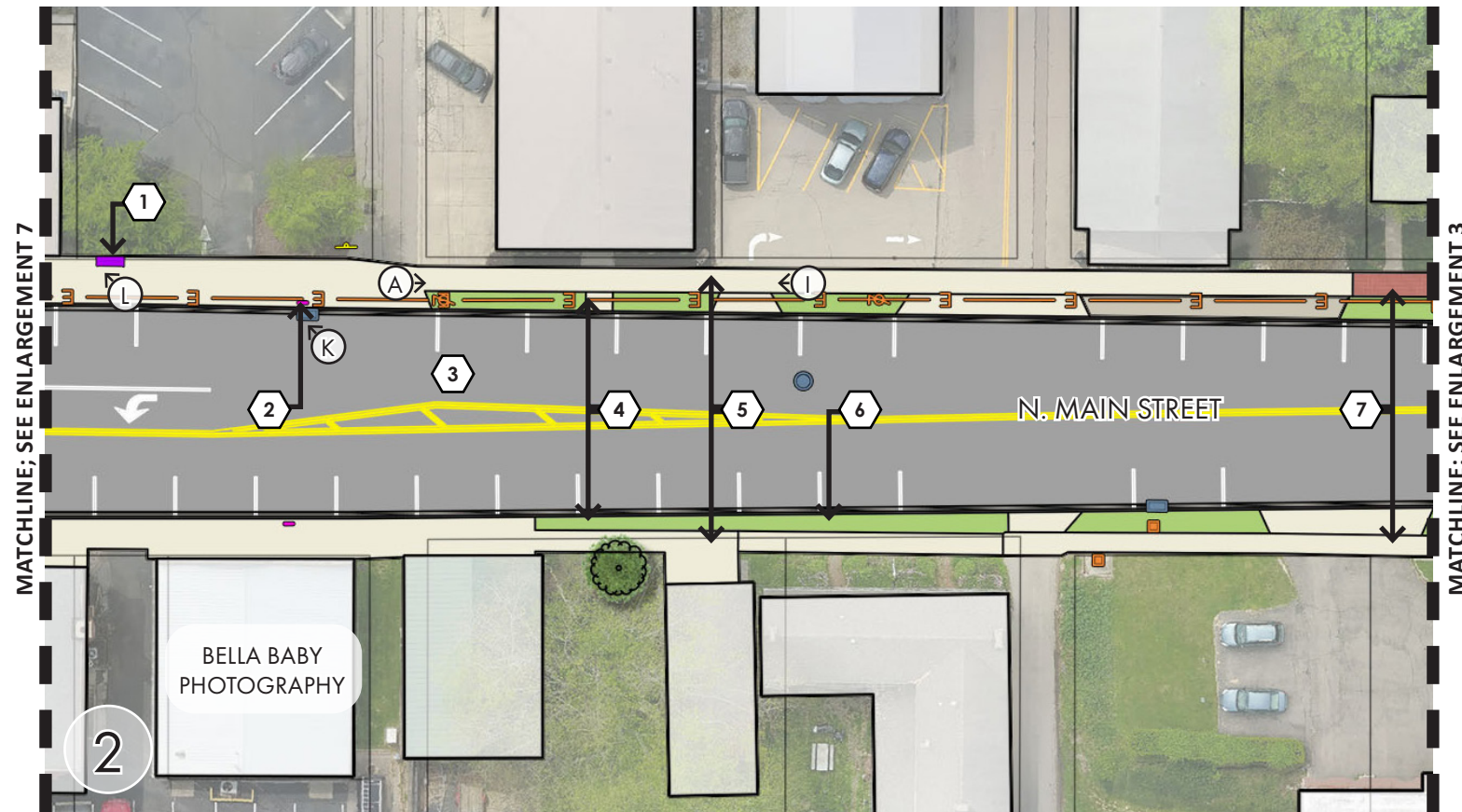
| | |
|---------------------------------|---|
| NO ADA WARNING PAD | ON-STREET PARKING ENCROACHING INTO DRIVE LANE |
| NARROW/DAMAGED SIDEWALK | |
| UNMARKED ON-STREET PARKING | |
| STREET TREE TO BE REMOVED, TYP. | |
| NO PEDESTRIAN SCALE LIGHTING | |
| INCONSISTENT STREET TREE CANOPY | |
| STEEP TREE LAWN | |

KEY MAP



Existing Downtown Streetscape Plan

SCALE: 1" = 40'-0"



LEGEND

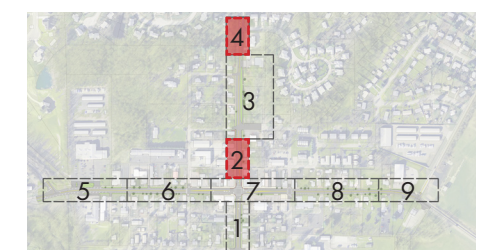
- | | | | |
|-------------------|--------------------------|------------|---------------------------------|
| CONCRETE SIDEWALK | OVERHEAD ELECTRICAL LINE | LIGHT POLE | STREET TREE TO BE REMOVED, TYP. |
| PAVERS | UTILITY POLE | BENCH | |
| TREE LAWN | FIRE HYDRANT | BIKE RACK | |
| ADA WARNING PAD | MANHOLE | PLANTER | |
| SIGN | STORM DRAIN | TRASH CAN | |

(A) → EXISTING CONDITION VIEW, SEE SHEETS 1.10 AND 1.11

PROPOSED KEY NOTES

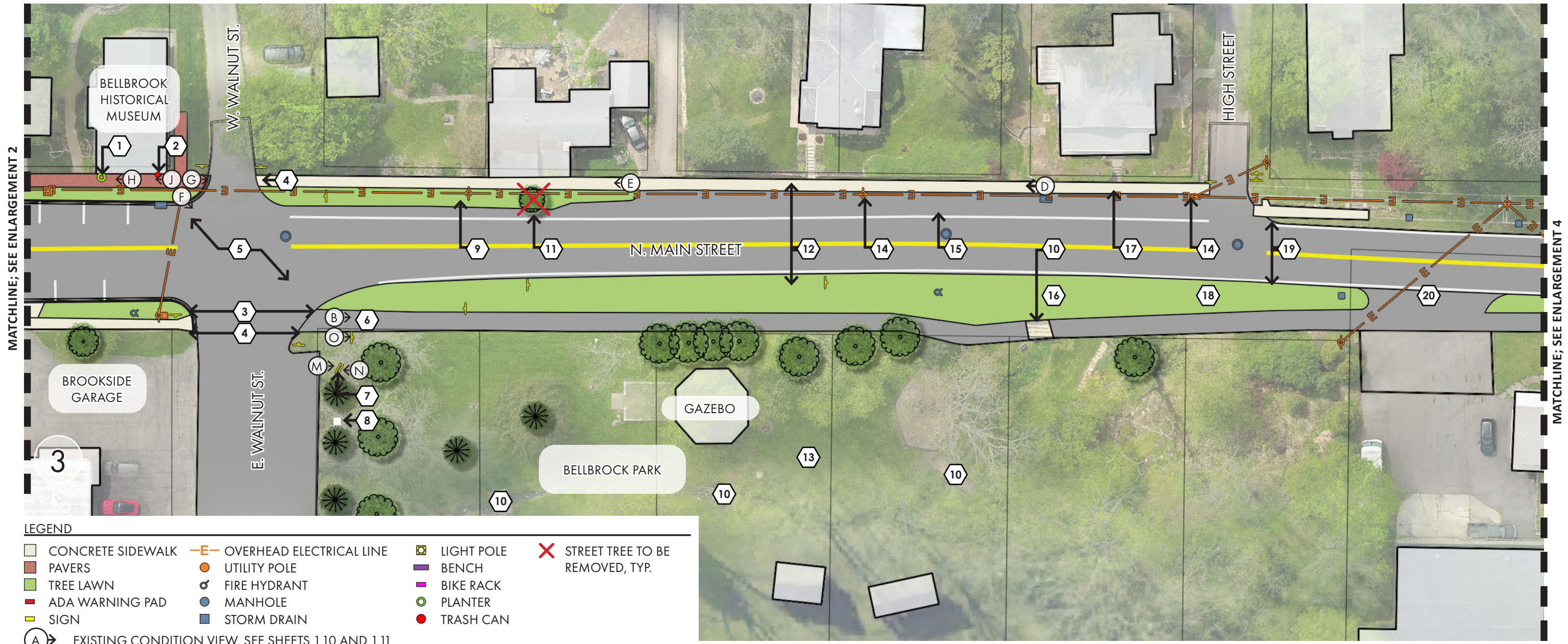
- | | |
|--|--|
| 1 BENCH, TYP. | 8 BELLBROOK RICHARD L. FREDERICK BIKEWAY |
| 2 BIKE RACK, TYP. | 9 STORM WATER CONCRETE FLUME |
| 3 LACK OF BIKE LANE MARKINGS/SIGNAGE | 10 NO CROSSWALK |
| 4 NO PEDESTRIAN LIGHTING | |
| 5 NARROW/DAMAGED SIDEWALK | |
| 6 INCONSISTENT STREET TREE CANOPY | |
| 7 INCONSISTENT SIDEWALK PAVING MATERIALS | |

KEY MAP



Existing Downtown Streetscape Plan

SCALE: 1" = 40'-0"



MATCHLINE; SEE ENLARGEMENT 2

MATCHLINE; SEE ENLARGEMENT 4

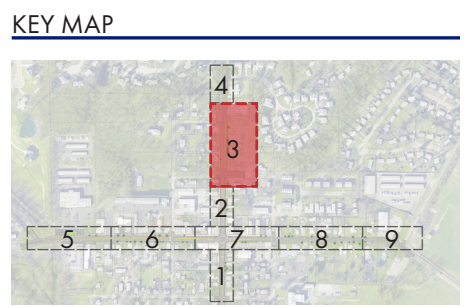
LEGEND

| | | | |
|-------------------|--------------------------|------------|---------------------------------|
| CONCRETE SIDEWALK | OVERHEAD ELECTRICAL LINE | LIGHT POLE | STREET TREE TO BE REMOVED, TYP. |
| PAVERS | UTILITY POLE | BENCH | |
| TREE LAWN | FIRE HYDRANT | BIKE RACK | |
| ADA WARNING PAD | MANHOLE | PLANTER | |
| SIGN | STORM DRAIN | TRASH CAN | |

EXISTING CONDITION VIEW, SEE SHEETS 1.10 AND 1.11

PROPOSED KEY NOTES

| | | | |
|--|-------------------------------------|---|---------------------|
| PLANTER, TYP. | RICHARD L. FREDERICK BIKEWAY BEGINS | NO ADA ACCESS THROUGH PARK | NO CURB |
| TRASH CAN, TYP. | MONUMENT SIGN WITH HISTORIC MARKER | UTILITY POLE IN ROAD | EXTRA WIDE CURB CUT |
| CURB RAMP IS NOT ADEQUATE WIDTH FOR PEDESTRIAN/CYCLIST TRAFFIC | COMMUNITY LIBRARY BOX | UNMARKED ON-STREET PARKING | |
| NO ADA WARNING PAD | INCONSISTENT STREET TREE CANOPY | CULVERT | |
| NO PEDESTRIAN CONNECTIVITY (CROSSWALKS) TO BELLBROCK PARK | BRIDGE | NARROW/DAMAGED SIDEWALK | |
| | STREET TREE TO BE REMOVED | STEEP SLOPE TO BIKE PATH WITHOUT BUFFER | |
| | NO PEDESTRIAN SCALE LIGHTING | | |



Existing Downtown Streetscape Plan

SCALE: 1" = 40'-0"



A. OVERHEAD UTILITY WIRES ARE PRESENT ALONG THE STREETScape



B. STEEP HILL WITH NO BUFFER BETWEEN BIKE PATH AND ROAD



C. STORMWATER CONCRETE FLUME



D. UTILITY POLE IN A PARKING SPACE



E. TREE TOO LARGE FOR ITS PLANTING LOCATION



F. NO CROSSWALKS TO BELLBROOK PARK



G. NO ADA WARNING MAT AT CROSSWALK



H. TREE LAWN OF TURF AND AGGREGATE MULCH

Existing Downtown Conditions Imagery



I. CURB CUTS LIMIT ON-STREET PARKING



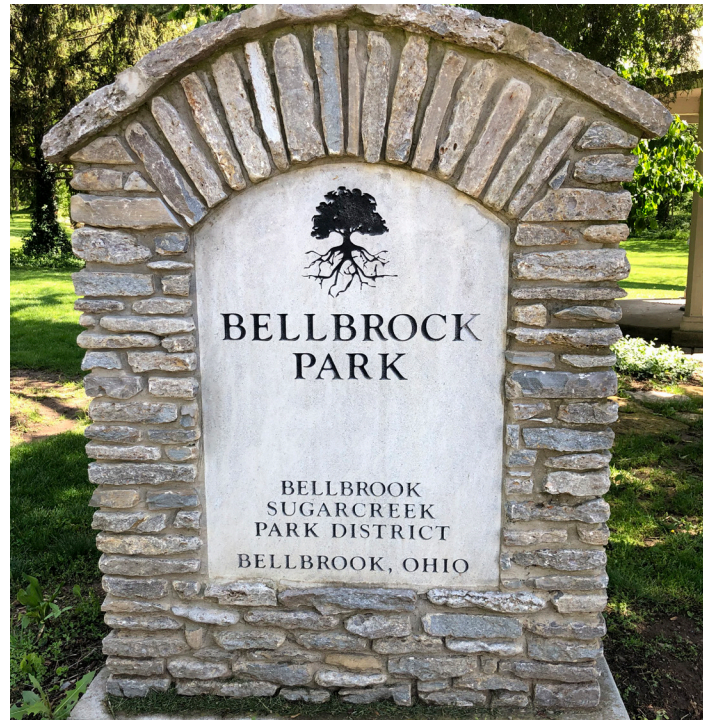
J. EXISTING LITTER RECEPTACLE



K. EXISTING BIKE RACKS PROVIDE BRANDING



L. EXISTING BENCHES



M. BELLBROOK PARK MONUMENT SIGN WITH HISTORICAL MARKER

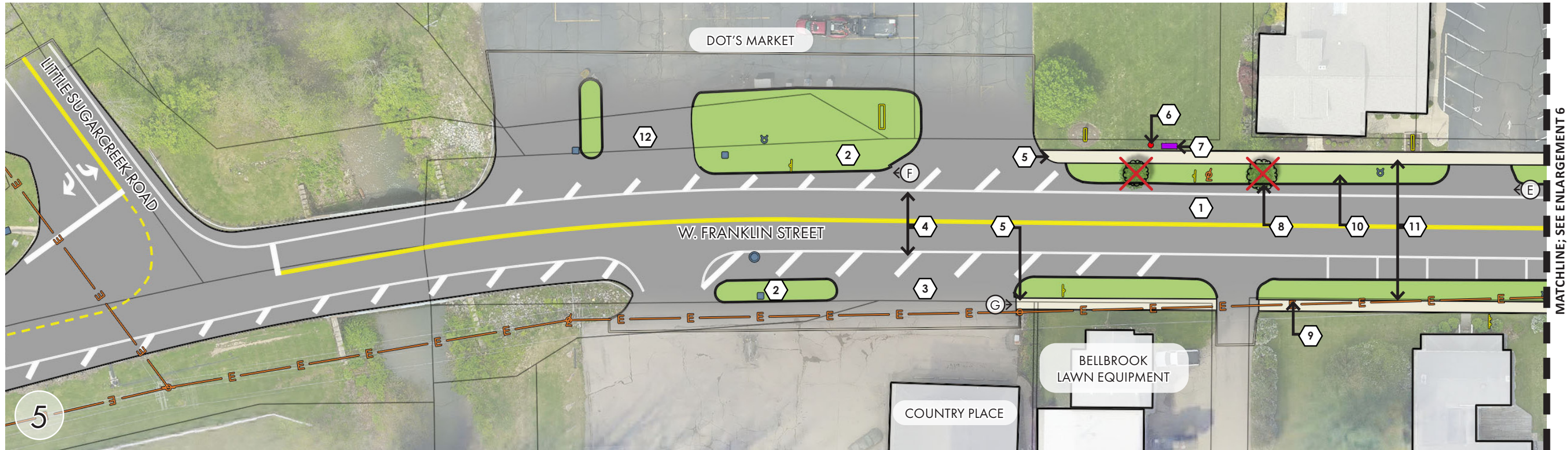


N. BELLBROOK PARK MONUMENT SIGN WITH HISTORICAL MARKER



O. BELLBROOK RICHARD L. FREDERICK BIKEWAY SIGN

Existing Downtown Conditions Imagery



MATCHLINE; SEE ENLARGEMENT 6

LEGEND

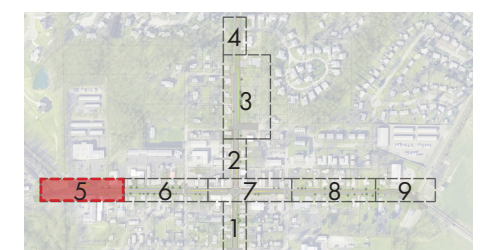
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|-------------------|--------------------------|------------|---------------------------------|
| CONCRETE SIDEWALK | OVERHEAD ELECTRICAL LINE | LIGHT POLE | STREET TREE TO BE REMOVED, TYP. |
| PAVERS | UTILITY POLE | BENCH | |
| TREE LAWN | FIRE HYDRANT | BIKE RACK | |
| ADA WARNING PAD | MANHOLE | PLANTER | |
| SIGN | STORM DRAIN | TRASH CAN | |

EXISTING CONDITION VIEW, SEE SHEETS 1.14

PROPOSED KEY NOTES

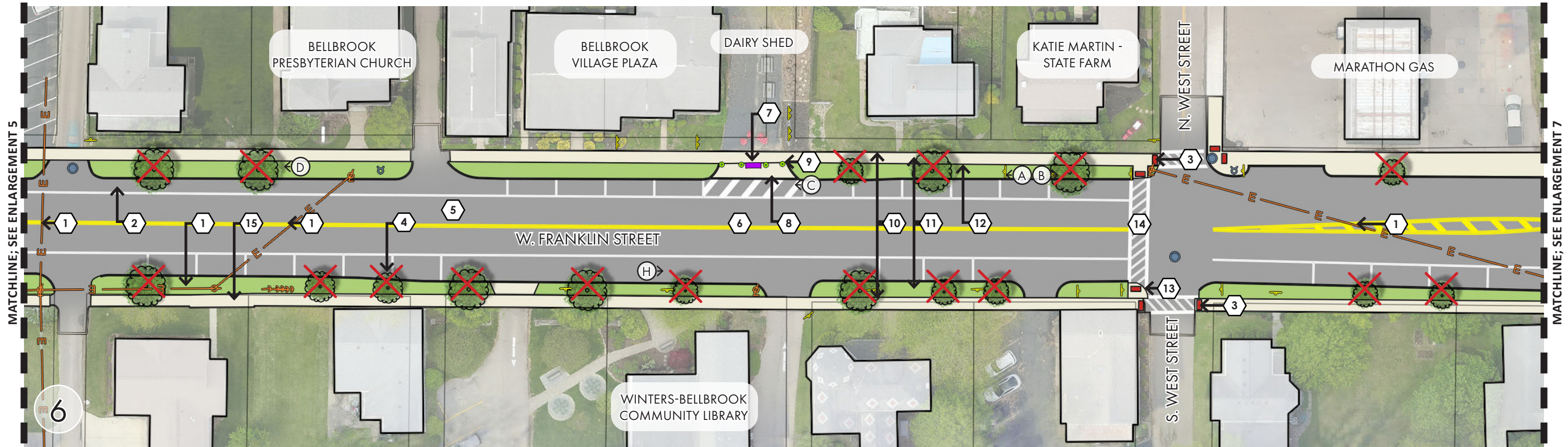
- | | |
|------------------------------------|---------------------------------|
| LACK OF BIKE LANE MARKINGS/SIGNAGE | STREET TREE TO BE REMOVED, TYP. |
| NO GATEWAY ELEMENT TO DOWNTOWN | OVERHEAD UTILITIES |
| EXTRA WIDE ACCESS POINT | INCONSISTENT STREET TREE CANOPY |
| EXTRA WIDE PAVEMENT | NARROW/DAMAGED SIDEWALK |
| CONCRETE SIDEWALK ENDS | REDUNDANT CURB CUT |
| TRASH CAN, TYP. | |
| BENCH, TYP. | |

KEY MAP



Existing Downtown Streetscape Plan

SCALE 1"=40'



LEGEND

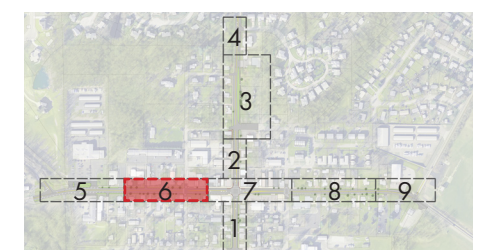
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|-------------------|--------------------------|------------|---------------------------------|
| CONCRETE SIDEWALK | OVERHEAD ELECTRICAL LINE | LIGHT POLE | STREET TREE TO BE REMOVED, TYP. |
| PAVERS | UTILITY POLE | BENCH | |
| TREE LAWN | FIRE HYDRANT | BIKE RACK | |
| ADA WARNING PAD | MANHOLE | PLANTER | |
| SIGN | STORM DRAIN | TRASH CAN | |

EXISTING CONDITION VIEW, SEE SHEETS 1.14

PROPOSED KEY NOTES

- | | | |
|--|---------------------------------------|---------------------------|
| OVERHEAD ELECTRICAL LINES | CURB CUT CLOSED TO VEHICLES | POOR DRAINAGE ON SIDEWALK |
| EXTRA WIDE PAVEMENT | PLANTER, TYP. | |
| ADA DETECTABLE WARNING PAD, TYP. | NARROW/DAMAGED SIDEWALK | |
| STREET TREE TO BE REMOVED, TYP. | NO PEDESTRIAN SCALE LIGHTING | |
| LACK OF BIKE LANE MARKINGS/SIGNAGE | INCONSISTENT STREET TREE CANOPY | |
| NO CROSSWALK AT ACTIVE PEDESTRIAN ZONE | DAMAGED CONCRETE AND ASPHALT PAVEMENT | |
| BENCH, TYP. | STRIPED CROSSWALK, TYP. | |

KEY MAP



Existing Downtown Streetscape Plan

SCALE 1"=40'



A. BIKE ROUTE SIGNAGE BUT NO STREET MARKINGS (SHARROWS)



B. ASPHALT CURB



C. UNUSED CURB CUT



D. DYING TREE



E. UNMARKED BIKE LANE (NO SHARROWS)



F. SIDEWALK ENDS

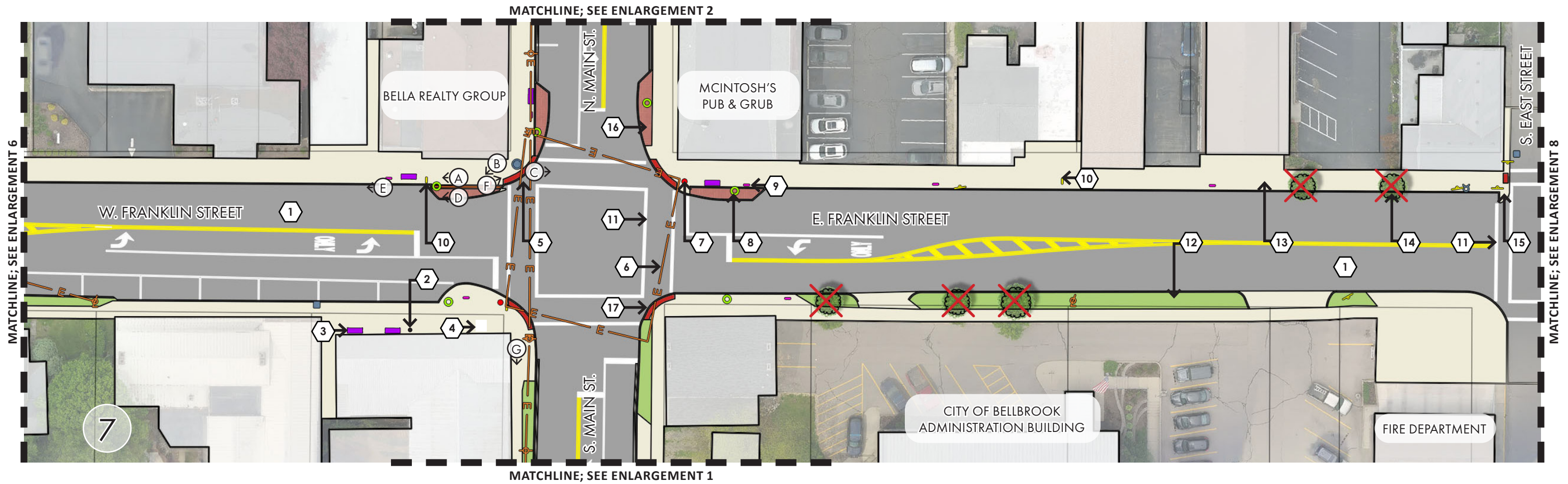


G. NO DEMARCATION OF WHERE PARKING BEGINS



H. OPPORTUNITY FOR MID BLOCK CROSSWALK TO CONNECT LIBRARY AND BELLBROOK PLAZA

Existing Downtown Conditions Imagery



LEGEND

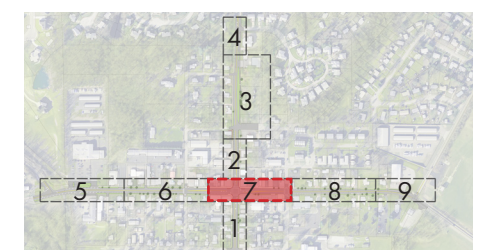
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|-------------------|--------------------------|------------|---------------------------------|
| CONCRETE SIDEWALK | OVERHEAD ELECTRICAL LINE | LIGHT POLE | STREET TREE TO BE REMOVED, TYP. |
| PAVERS | UTILITY POLE | BENCH | |
| TREE LAWN | FIRE HYDRANT | BIKE RACK | |
| ADA WARNING PAD | MANHOLE | PLANTER | |
| SIGN | STORM DRAIN | TRASH CAN | |

(A) → EXISTING CONDITION VIEW, SEE SHEETS 1.17

PROPOSED KEY NOTES

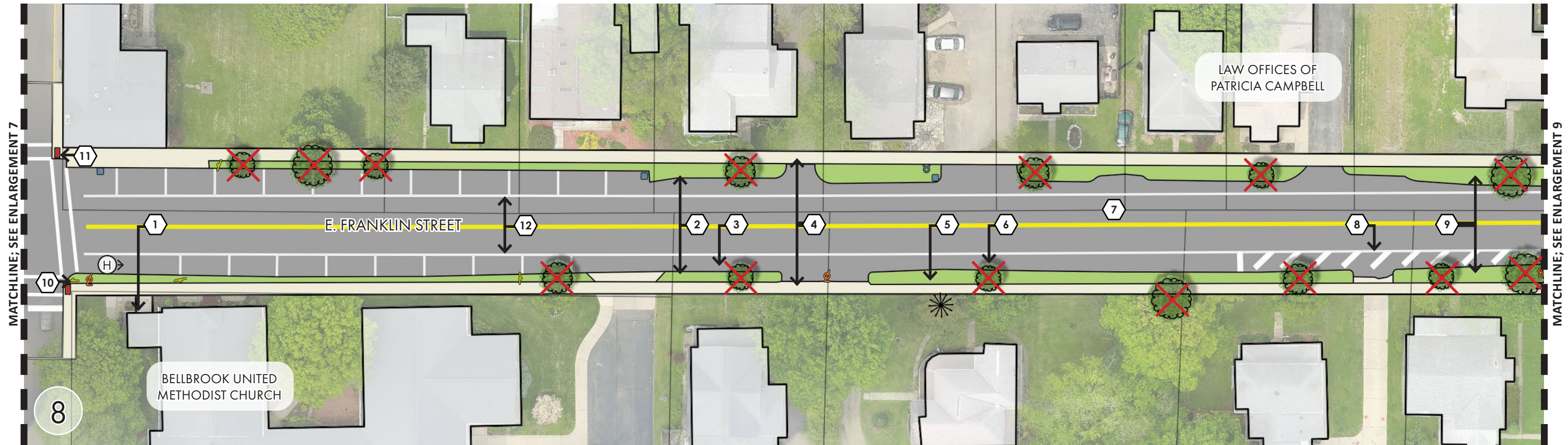
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|--------------------------------------|---|-------------------------------------|
| 1 LACK OF BIKE LANE MARKINGS/SIGNAGE | 8 EVERGREEN TREES IN PLANTERS BLOCK SIGHT | 14 STREET TREE TO BE REMOVED, TYP. |
| 2 HISTORIC WATER PUMP | 9 BIKE RACK, TYP. | 15 NON ADA COMPLIANT CURB RAMP |
| 3 BENCH, TYP. | 10 BUSINESS SIGN, TYP. | 16 DECORATIVE PAVERS, TYP. |
| 4 HISTORIC WAGON WHEELS | 11 STRIPED CROSSWALK, TYP. | 17 ADA DETECTABLE WARNING PAD, TYP. |
| 5 DRAINAGE ISSUES | 12 NO PEDESTRIAN SCALE LIGHTING | |
| 6 SPAN WIRE TRAFFIC SIGNALS | 13 INCONSISTENT STREET TREE CANOPY | |
| 7 TRASH CAN, TYP. | | |

KEY MAP



Existing Downtown Streetscape Plan

SCALE 1"=40'



LEGEND

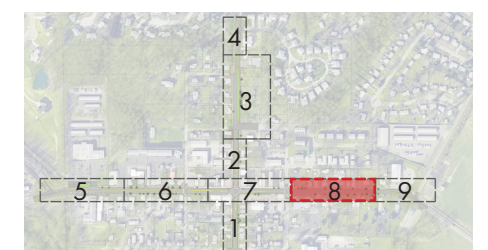
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| PAVERS | UTILITY POLE | BENCH | |
| TREE LAWN | FIRE HYDRANT | BIKE RACK | |
| ADA WARNING PAD | MANHOLE | PLANTER | |
| SIGN | STORM DRAIN | TRASH CAN | |

(A) → EXISTING CONDITION VIEW, SEE SHEETS 1.17

PROPOSED KEY NOTES

- | | |
|--|-------------------------------------|
| 1 HISTORIC MARKER | 8 EXTRA WIDE PAVEMENT |
| 2 NO CURB | 9 OPEN DRAINAGE DITCH ALONG ROADWAY |
| 3 NO ON-STREET PARKING PAVEMENT MARKINGS | 10 NO CURB RAMP |
| 4 NARROW/DAMAGED SIDEWALK | 11 ADA DETECTABLE WARNING PAD, TYP. |
| 5 INCONSISTENT STREET TREE CANOPY | 12 STRIPED ON-STREET PARKING |
| 6 STREET TREE TO BE REMOVED, TYP. | |
| 7 LACK OF BIKE LANE MARKINGS/SIGNAGE | |

KEY MAP



Existing Downtown Streetscape Plan

↑ SCALE 1"=40'



A. BUSINESS SIGN IN PUBLIC SPACE AND EVERGREEN TREES IN PLANTERS BLOCK SIGHT DISTANCE VIEWS



B. STORMWATER CHANNLED TO CROSSWALK



C. CROSSWALKS DELINEATED BY PARALLEL STRIPES



D. END OF BUMP OUT WIDENS THE STREET



E. CURB CUTS PROHIBIT ON STREET PARKING



F. DECORATIVE PAVERS AT INTERSECTION

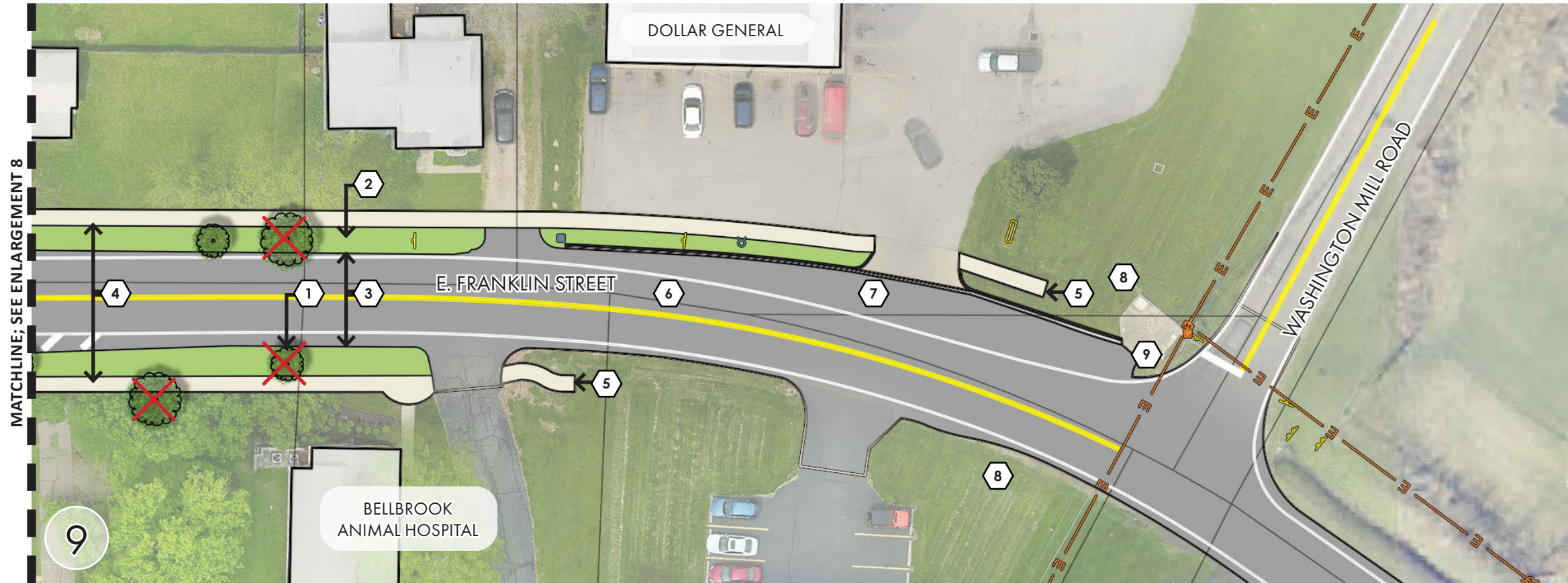


G. OVERHEAD UTILITIES PREVENT PLANTING STREET TREES



H. NO CURB

Existing Downtown Conditions Imagery



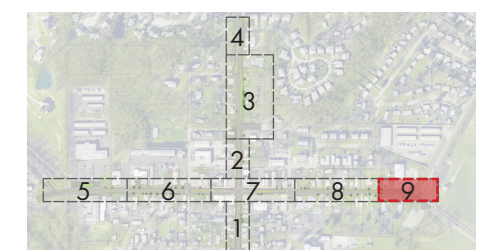
LEGEND

| | | | |
|-------------------|--------------------------|------------|---------------------------------|
| CONCRETE SIDEWALK | OVERHEAD ELECTRICAL LINE | LIGHT POLE | STREET TREE TO BE REMOVED, TYP. |
| PAVERS | UTILITY POLE | BENCH | |
| TREE LAWN | FIRE HYDRANT | BIKE RACK | |
| ADA WARNING PAD | MANHOLE | PLANTER | |
| SIGN | STORM DRAIN | TRASH CAN | |

PROPOSED KEY NOTES

| | |
|---|-----------------------|
| STREET TREE TO BE REMOVED, TYP. | EXTRA WIDE PAVEMENT |
| INCONSISTENT STREET TREE CANOPY | NO GATEWAY ELEMENT |
| NO CURB | MANHOLE DRAINAGE PIPE |
| NARROW/DAMAGED SIDEWALK | |
| CONCRETE SIDEWALK ENDS WITH NO ACCESS TO WASHINGTON MILL ROAD | |
| LACK OF BIKE LANE MARKINGS/SIGNAGE | |

KEY MAP



Existing Downtown Streetscape Plan

↑ SCALE 1"=40'

Proposed Downtown Streetscape Summary

Safety

- Along West Franklin Street provide a raised mid-block aesthetic crosswalk with flashing beacons at the Winters Bellbrook Library and the Dairy Shed.
- Provide sharrows and bike signage along Franklin Street and Main Street to notify drivers of potential cyclists.
- Remove on-street parking at the southeast corner of the Main and Franklin Street intersection to allow for proper vehicular stacking at the intersection.
- Provide shorter plantings in the planters to avoid blocking views.
- Provide pedestrian connectivity across Main Street and Walnut Street to Bellbrook Park.
- Ensure all crosswalks have ADA compliant curb ramps.
- Provide pedestrian scale lighting along the streetscape.

ADA Compliance

- Provide ADA accessible curb ramps and detectable warning pads at all crosswalk and sidewalk intersections.
- Provide a larger ADA accessible curb ramp at the northeast corner of N. Main and Walnut Street for a bike path.

History

- Provide an ADA accessible plaza area to celebrate the historic Bellbrook Park monument sign.
- New historical markers could be placed in front of buildings along the streetscape for better pedestrian and vehicular visibility.

Streetscape Experience

- New street trees along Main Street will create separation from the roadway and the bike path, making pedestrians and cyclists more comfortable.
- Provide additional sidewalk pedestrian amenity space for dining and resting areas along the streetscape.

Pedestrian Connectivity

- Along West Franklin Street provide a raised mid-block crosswalk with flashing beacons at the Winters Bellbrook Library and the Dairy Shed.
- On Franklin Street extend the sidewalks east to the intersection of Washington Mill Road and west to Little Sugarcreek Road.
- Provide aesthetic crosswalk markings such as piano key/ladder markings or painted asphalt to enhance visibility and safety.

Pedestrian Connectivity Cont.

- Provide crosswalks to create pedestrian connectivity across N. Main Street and Walnut Street to Bellbrook Park.
- Provide a wider curb ramp to accommodate the Richard L. Frederick Bikeway along Main Street.
- Provide crosswalks at the intersection of E. Franklin St. and Washington Mill Road and Little Sugarcreek Road and W. Franklin Street.

Streetscape Amenities

- Provide more City branded bike racks along the streetscape and near Bellbrook Park.
- Provide more benches to create more frequent resting areas along the streetscape.
- Add rain covers to existing litter receptacles and provide additional trash and recycling receptacles.
- Provide additional locations for streetscape furnishings in strategic locations along the streetscape.
- Provide additional planter locations along the streetscape.
- Planting material for the planters shall ensure open views for pedestrian safety.
- Provide for additional street amenities to include tables, art, and sculpture opportunities.

Wayfinding and Signage

- Provide gateway signage welcoming travelers into the downtown streetscape.
- Provide wayfinding and information signage throughout the downtown streetscape.

Branding

- New banner flags on pedestrian street lights create branding opportunities.
- New gateway elements to be installed on Franklin St. near Dot's Market and Washington Mill Rd.

Parking

- 119 proposed on street parking spaces.
- On-street parking spaces should be clearly marked on the pavement.
- Add parking signs to delineate where parking is allowed and restricted in the Downtown District.

Roadway

- Provide sharrows, shared lane markings, in the roadway and bike signage along Franklin Street and Main Street to alert drivers of potential cyclists.
- Narrow the wide shoulders to create more pedestrian streetscape amenity space and to help lower speeds on the roadway.
- Narrow access points into driveways where it won't interfere with truck traffic to gain more pedestrian amenity area.
- Provide concrete curbs where they are asphalt, damaged, or absent.
- Stop bars to be placed 8' from crosswalks for added safety.

Proposed Downtown Streetscape Summary

Street Trees

- Provide a consistent tree canopy along the streetscape to enhance pedestrian comfort.
- Remove overgrown and dying trees.
- Proposed tree types shall tolerate the urban environment and limit sidewalk damage caused by roots.

Drainage

- Installation of storm sewers would be beneficial in creating more pedestrian amenity space and create an attractive streetscape environment.

Lighting

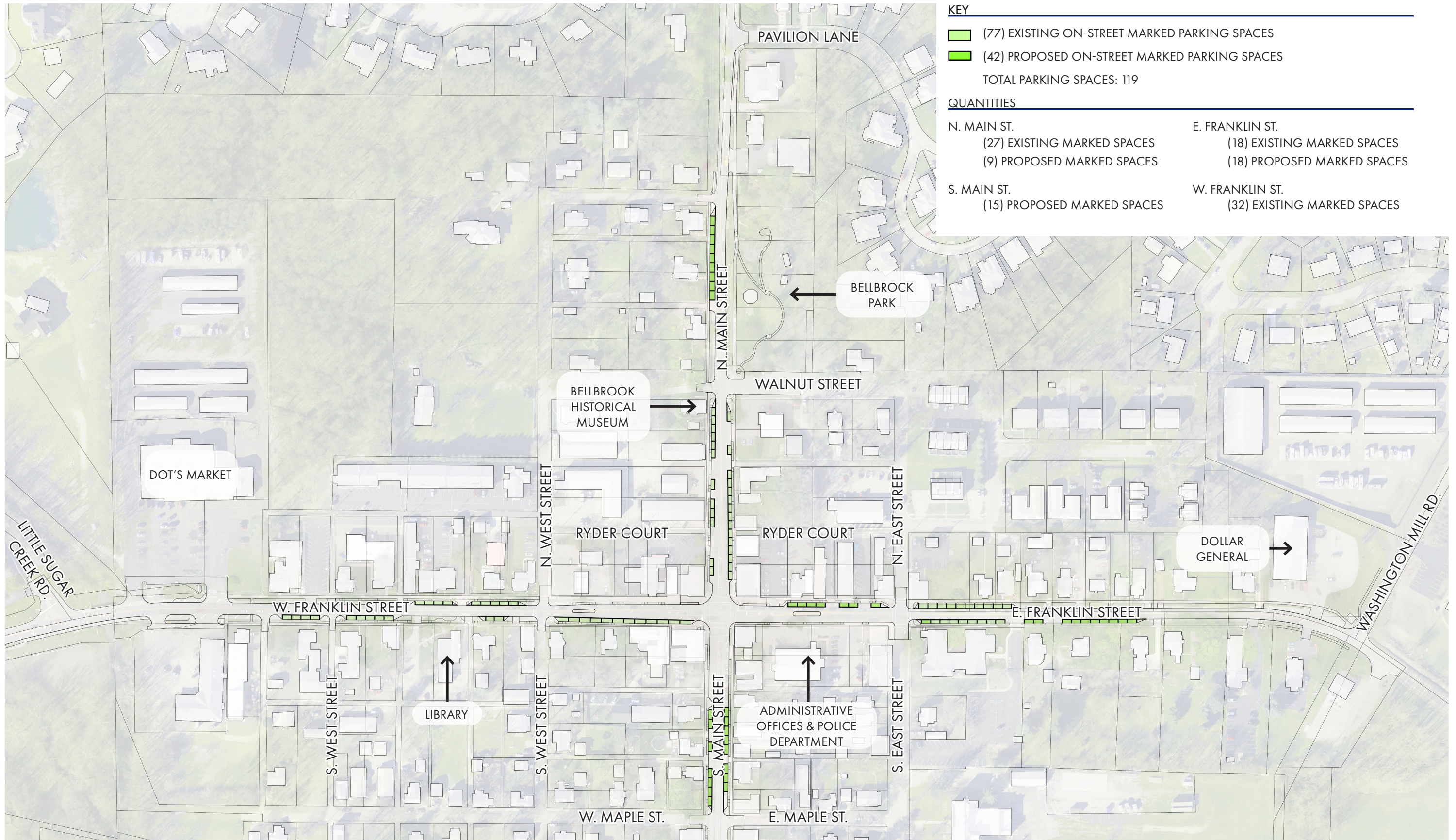
- Provide pedestrian scale lighting to improve the pedestrian safety, increase streetscape walkable hours, and aesthetics.
- Pedestrian scale lighting shall allow for City branded banners and electrical outlets for holiday décor.
- Provide aesthetic vehicular roadway lighting.

Utilities

- Remove overhead utilities and provide for underground utilities. If overhead utility wires remain, provide for a short tree variety to be placed underneath.
- Provide mast arm poles for intersection signals.

Sidewalk Pavement

- Provide new concrete sidewalks to create a consistent level pavement, width, and jointing pattern.
- Eliminate the tree lawn and expand the sidewalk to provide pedestrian amenity space, pavers and trees in grates.



KEY

- (77) EXISTING ON-STREET MARKED PARKING SPACES
- (42) PROPOSED ON-STREET MARKED PARKING SPACES

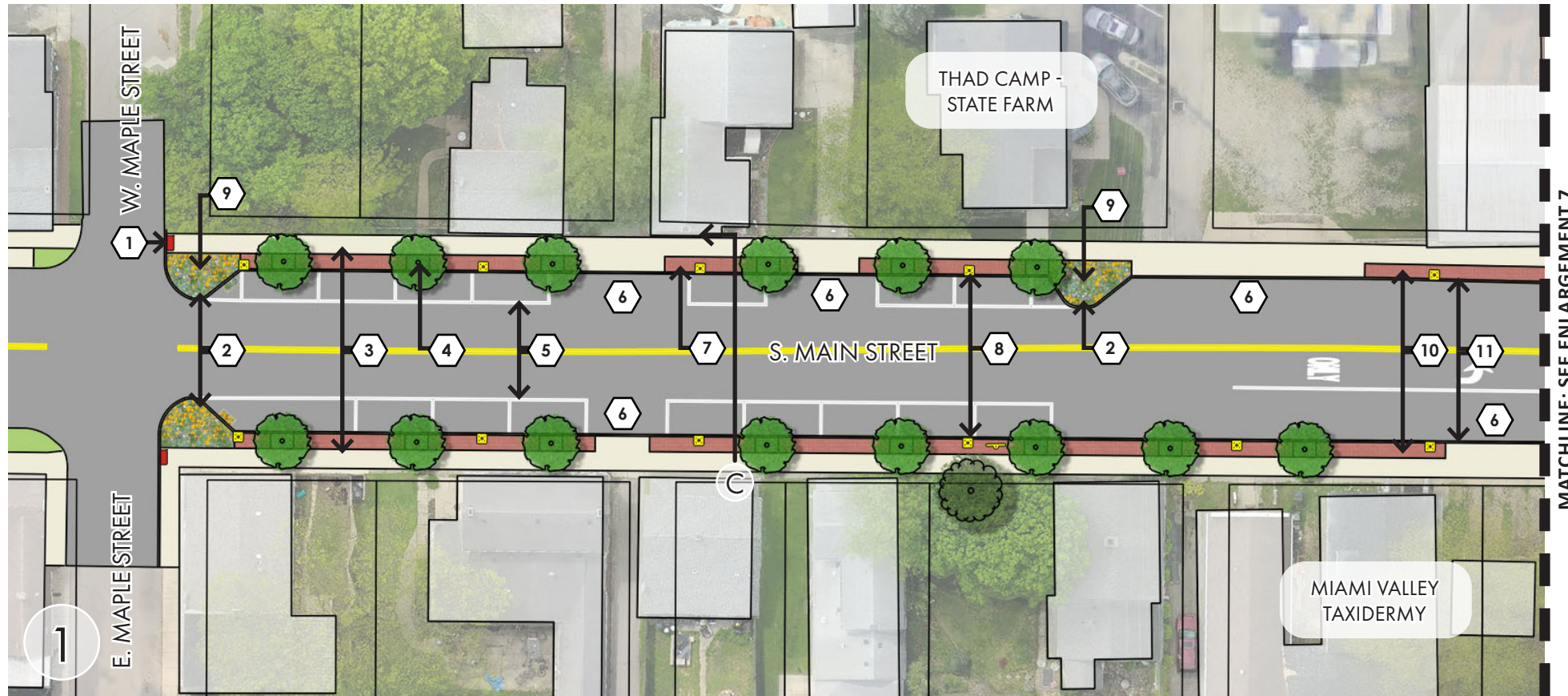
TOTAL PARKING SPACES: 119

QUANTITIES

| | |
|--|---|
| N. MAIN ST. (27) EXISTING MARKED SPACES (9) PROPOSED MARKED SPACES | E. FRANKLIN ST. (18) EXISTING MARKED SPACES (18) PROPOSED MARKED SPACES |
| S. MAIN ST. (15) PROPOSED MARKED SPACES | W. FRANKLIN ST. (32) EXISTING MARKED SPACES |

Proposed Downtown Streetscape Parking

↑ SCALE: 1"=200'-0"



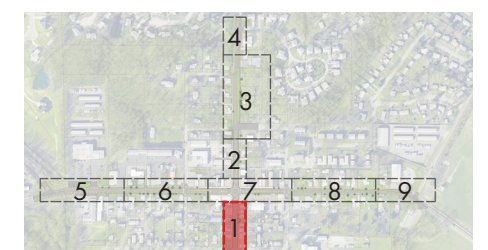
LEGEND

- | | |
|----------------------------|------------------------|
| CONCRETE SIDEWALK | NEW BENCH LOCATION |
| PAVERS | NEW BIKE RACK LOCATION |
| TREE LAWN | NEW PLANTER |
| ADA WARNING PAD | NEW TRASH CAN |
| SIGN | FIRE HYDRANT |
| RAPID FLASHING BEACON | MANHOLE |
| NEW LIGHT POLE WITH BANNER | STORM DRAIN |
- A GRAPHIC SECTION CUT SEE SHEET 2.12

PROPOSED KEY NOTES

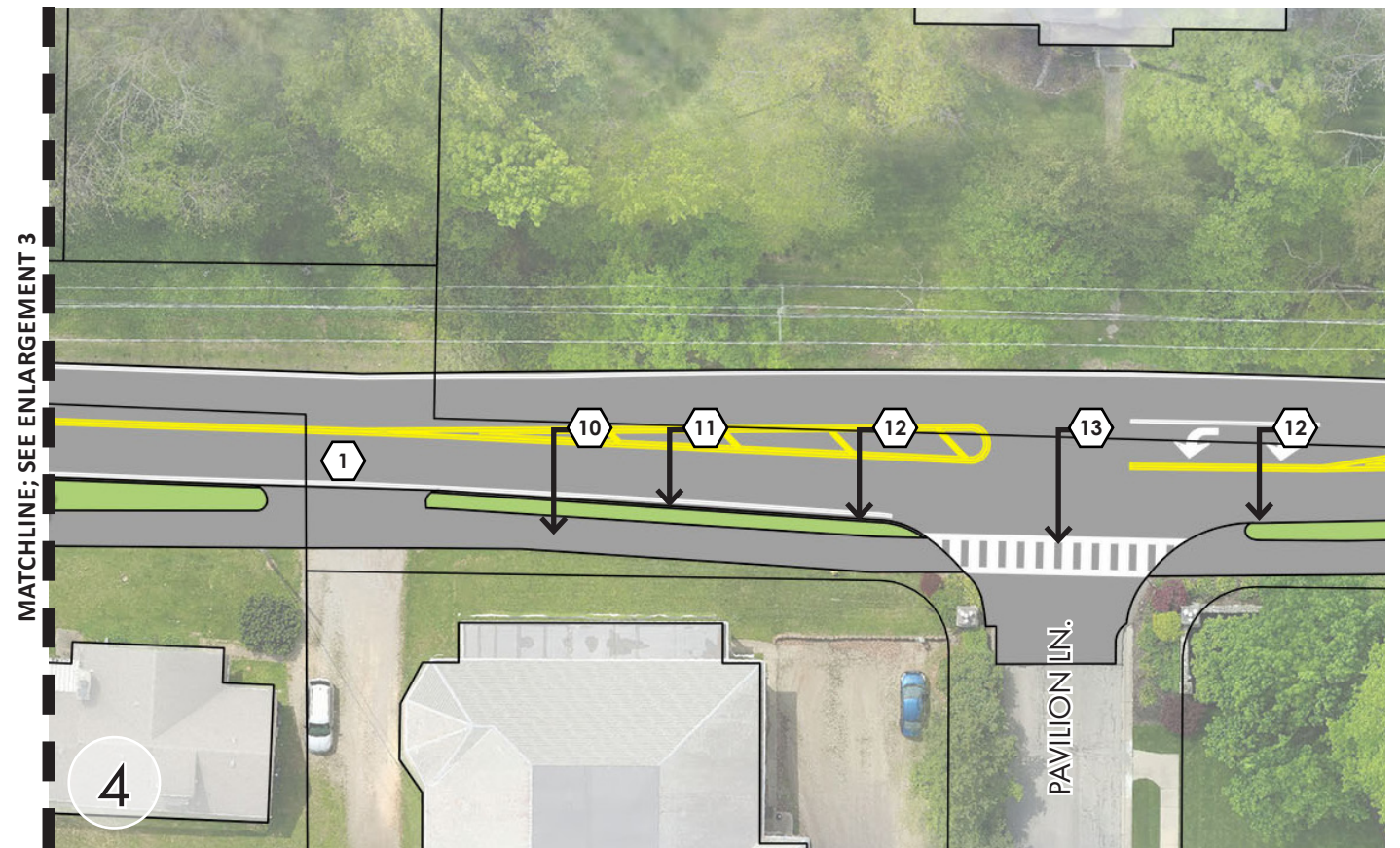
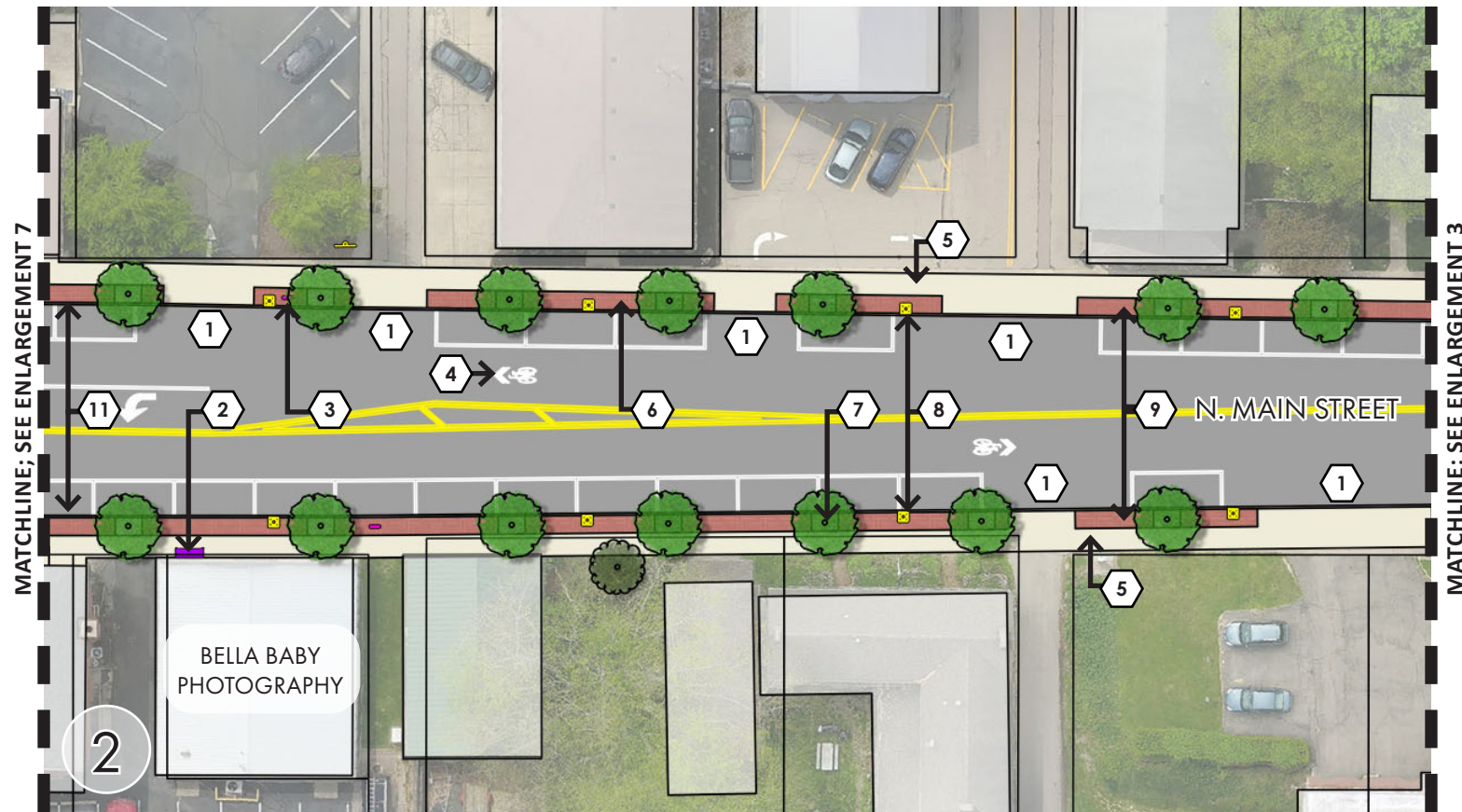
- | | |
|--|---|
| 1 PROPOSED ADA DETECTABLE WARNING PAD, TYP. | 7 PROPOSED UNDERGROUND UTILITIES |
| 2 PROPOSED CURB BUMP OUT, TYP. | 8 NEW PEDESTRIAN LIGHT POLE WITH BANNER AND ELECTRICAL OUTLET, TYP. |
| 3 PROPOSED 5' CONCRETE SIDEWALK | 9 PROPOSED PLANT BED, TYP. |
| 4 PROPOSED STREET TREE IN GRATE WITH ELECTRICAL OUTLET, TYP. | 10 PROPOSED 4' DECORATIVE PAVER BANDING, TYP. |
| 5 PROPOSED ON-STREET PARKING STRIPING, TYP. | 11 PROPOSED 6" CONCRETE CURB, TYP. |
| 6 PROPOSED CURB CUT | |

KEY MAP



Proposed Downtown Streetscape Plan

SCALE: 1" = 40'-0"

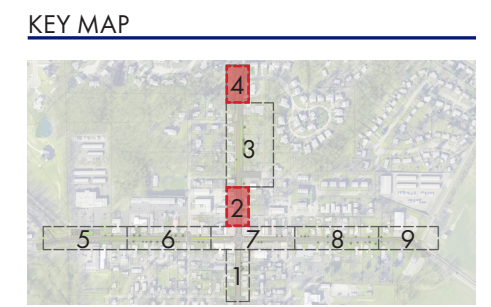


LEGEND

| | |
|----------------------------|------------------------|
| CONCRETE SIDEWALK | NEW BENCH LOCATION |
| PAVERS | NEW BIKE RACK LOCATION |
| TREE LAWN | NEW PLANTER |
| ADA WARNING PAD | NEW TRASH CAN |
| SIGN | FIRE HYDRANT |
| RAPID FLASHING BEACON | MANHOLE |
| NEW LIGHT POLE WITH BANNER | STORM DRAIN |

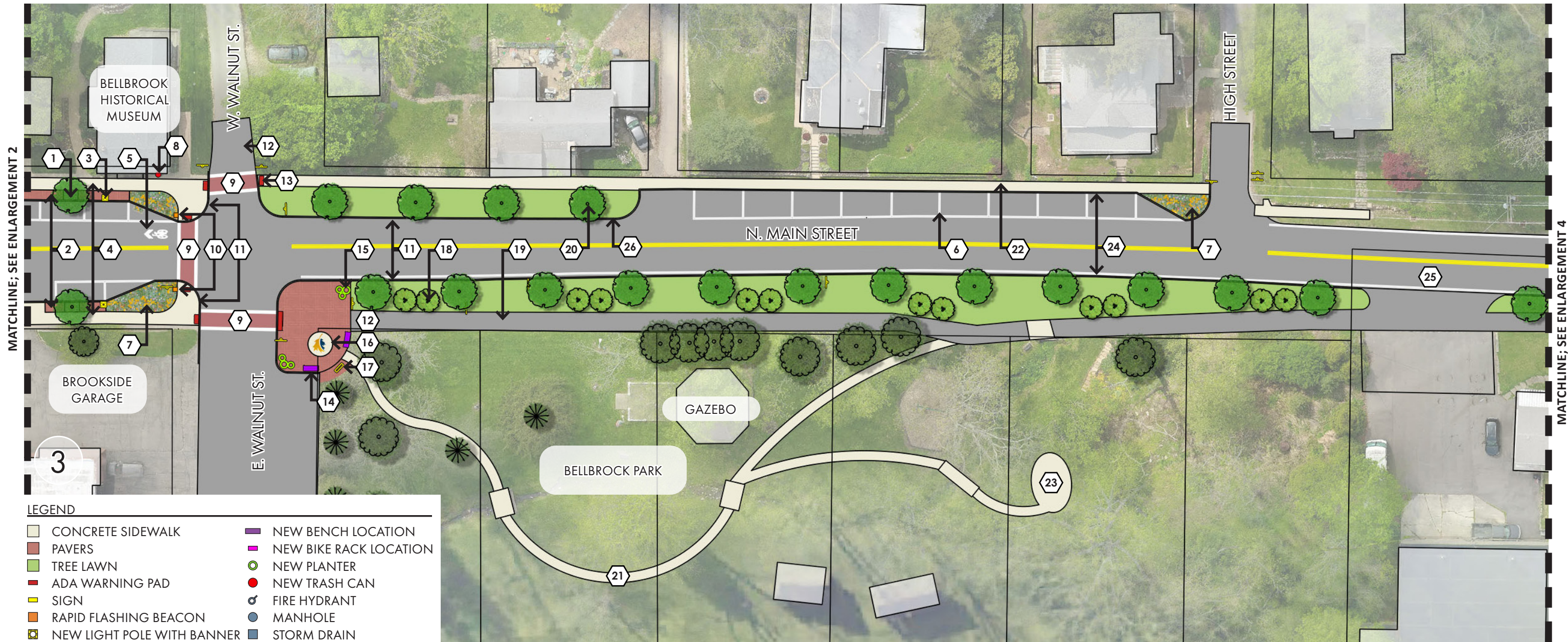
PROPOSED KEY NOTES

| | | |
|---|---|------------------------------|
| PROPOSED CURB CUT | PROPOSED STREET TREE IN GRATE WITH ELECTRICAL OUTLET, TYP. | PROPOSED 6" CONCRETE CURB |
| PROPOSED AND/OR RELOCATED BENCH, TYP. | NEW PEDESTRIAN LIGHT POLE WITH BANNER AND ELECTRICAL OUTLET, TYP. | PROPOSED STORM SEWER |
| PROPOSED AND/OR RELOCATED BIKE RACK, TYP. | PROPOSED 4' DECORATIVE PAVER BANDING, TYP. | PROPOSED PIANO KEY CROSSWALK |
| PROPOSED SHARROW PAVEMENT MARKING, TYP. | RICHARD L. FREDERICK BIKEWAY | |
| PROPOSED 5' CONCRETE SIDEWALK | | |
| PROPOSED UNDERGROUND UTILITIES | | |



Proposed Downtown Streetscape Plan

SCALE: 1" = 40'-0"



MATCHLINE; SEE ENLARGEMENT 2

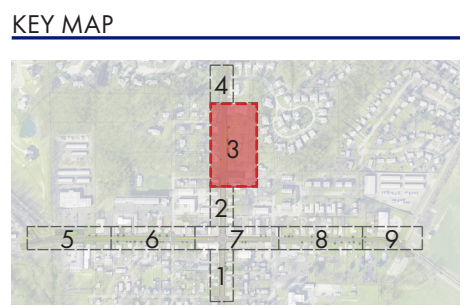
MATCHLINE; SEE ENLARGEMENT 4

LEGEND

| | |
|----------------------------|------------------------|
| CONCRETE SIDEWALK | NEW BENCH LOCATION |
| PAVERS | NEW BIKE RACK LOCATION |
| TREE LAWN | NEW PLANTER |
| ADA WARNING PAD | NEW TRASH CAN |
| SIGN | FIRE HYDRANT |
| RAPID FLASHING BEACON | MANHOLE |
| NEW LIGHT POLE WITH BANNER | STORM DRAIN |

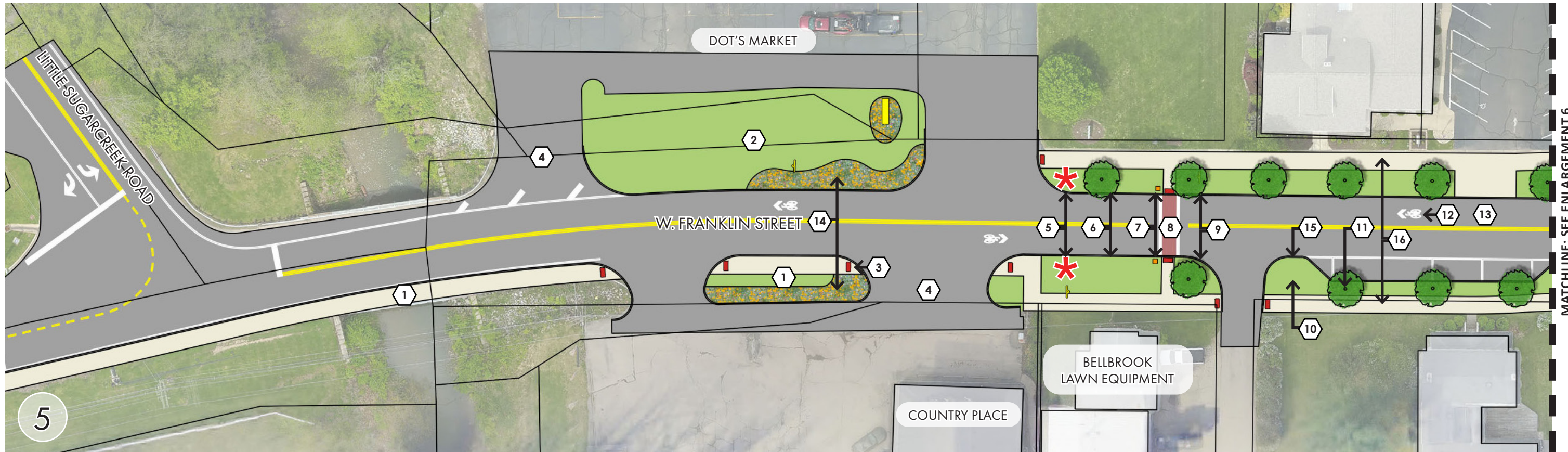
PROPOSED KEY NOTES

| | | | |
|--|---|---|--|
| 1 PROPOSED STREET TREE IN GRATE WITH ELECTRICAL OUTLET, TYP. | 7 PROPOSED PLANT BED, TYP. | 13 PROPOSED ADA DETECTABLE WARNING PAD, TYP. | 20 PROPOSED STREET TREE, TYP. |
| 2 PROPOSED 4' DECORATIVE PAVER BANDING | 8 PROPOSED TRASH CAN, TYP. | 14 PROPOSED AND/OR RELOCATED BENCH, TYP. | 21 PROPOSED ADA PEDESTRIAN CONNECTIVITY |
| 3 NEW PEDESTRIAN LIGHT POLE WITH BANNER AND ELECTRICAL OUTLET | 9 PROPOSED DECORATIVE CROSSWALK | 15 PROPOSED PLANTERS, TYP. | 22 PROPOSED UNDERGROUND UTILITIES |
| 4 PROPOSED 5' CONCRETE SIDEWALK | 10 PROPOSED RAPID FLASHING BEACON | 16 PROPOSED PLAZA WITH BRANDING ELEMENT | 23 PROPOSED SEATING AREA |
| 5 PROPOSED SHARROW PAVEMENT MARKING | 11 PROPOSED 6" CONCRETE CURB | 17 MONUMENT SIGN WITH HISTORIC MARKER | 24 PROPOSED STORM SEWER |
| 6 PROPOSED ON-STREET PARKING STRIPING, TYP. | 12 PROPOSED BIKEWAY CONNECTION FROM BELLBROOK PARK TO W. WALNUT STREET FOR FUTURE BELLBROOK-FAIRBORN CONNECTOR BIKEWAY | 18 ADDITIONAL LANDSCAPE BUFFER ALONG BIKE PATH | 25 PROPOSED CURB CUT |
| | | 19 RICHARD L. FREDERICK BIKEWAY | 26 PROPOSED CURB BUMP OUT, TYP. |



Proposed Downtown Streetscape Plan

N SCALE: 1" = 40'-0"



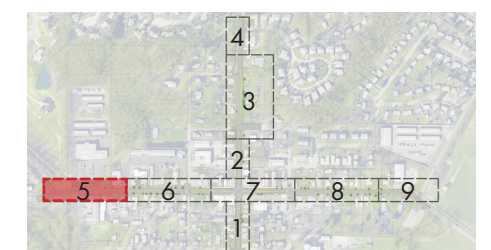
LEGEND

- | | |
|----------------------------|------------------------|
| CONCRETE SIDEWALK | NEW BENCH LOCATION |
| PAVERS | NEW BIKE RACK LOCATION |
| TREE LAWN | NEW PLANTER |
| ADA WARNING PAD | NEW TRASH CAN |
| SIGN | FIRE HYDRANT |
| RAPID FLASHING BEACON | MANHOLE |
| NEW LIGHT POLE WITH BANNER | STORM DRAIN |

PROPOSED KEY NOTES

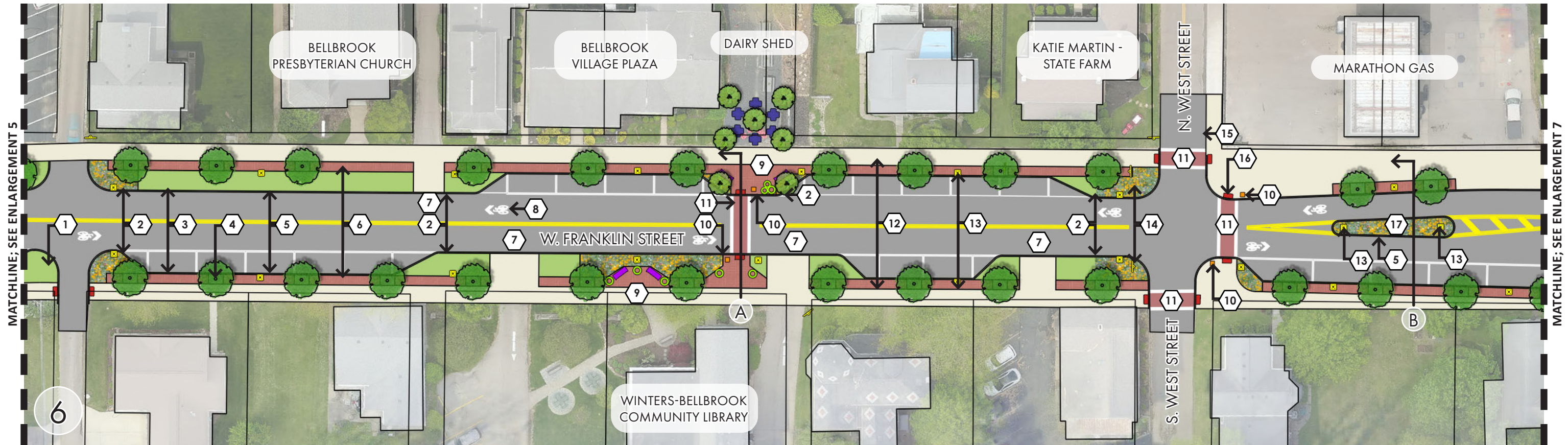
- | | | |
|---|---|---|
| 1 SIDEWALK & ROADWAY IMPROVEMENTS PER PROPOSED BRIDGE PLAN | 7 PROPOSED RAPID FLASHING BEACON | 14 PROPOSED PLANT BED, TYP. |
| 2 REVISED LANDSCAPE ISLAND | 8 PROPOSED DECORATIVE CROSSWALK | 15 PROPOSED CURB BUMP OUT, TYP. |
| 3 PROPOSED ADA DETECTABLE WARNING PAD, TYP. | 9 PROPOSED STORM SEWER | 16 PROPOSED 7' CONCRETE SIDEWALK, TYP. |
| 4 ACCESS POINT WITH REDUCED WIDTH | 10 PROPOSED UNDERGROUND UTILITIES | |
| 5 PROPOSED GATEWAY ELEMENT | 11 PROPOSED STREET TREE, TYP. | |
| 6 PROPOSED 6" CONCRETE CURB | 12 PROPOSED SHARROW PAVEMENT MARKING, TYP. | |
| | 13 PROPOSED CURB CUT | |

KEY MAP



Proposed Downtown Streetscape Plan

↑ SCALE 1"=40'



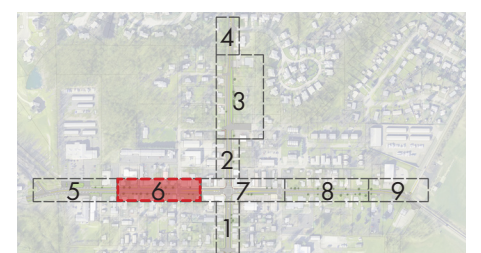
LEGEND

- CONCRETE SIDEWALK
 - PAVERS
 - TREE LAWN
 - ADA WARNING PAD
 - SIGN
 - RAPID FLASHING BEACON
 - NEW LIGHT POLE WITH BANNER
 - NEW BENCH LOCATION
 - NEW BIKE RACK LOCATION
 - NEW PLANTER
 - NEW TRASH CAN
 - FIRE HYDRANT
 - MANHOLE
 - STORM DRAIN
- A GRAPHIC SECTION CUT SEE SHEET 2.12

PROPOSED KEY NOTES

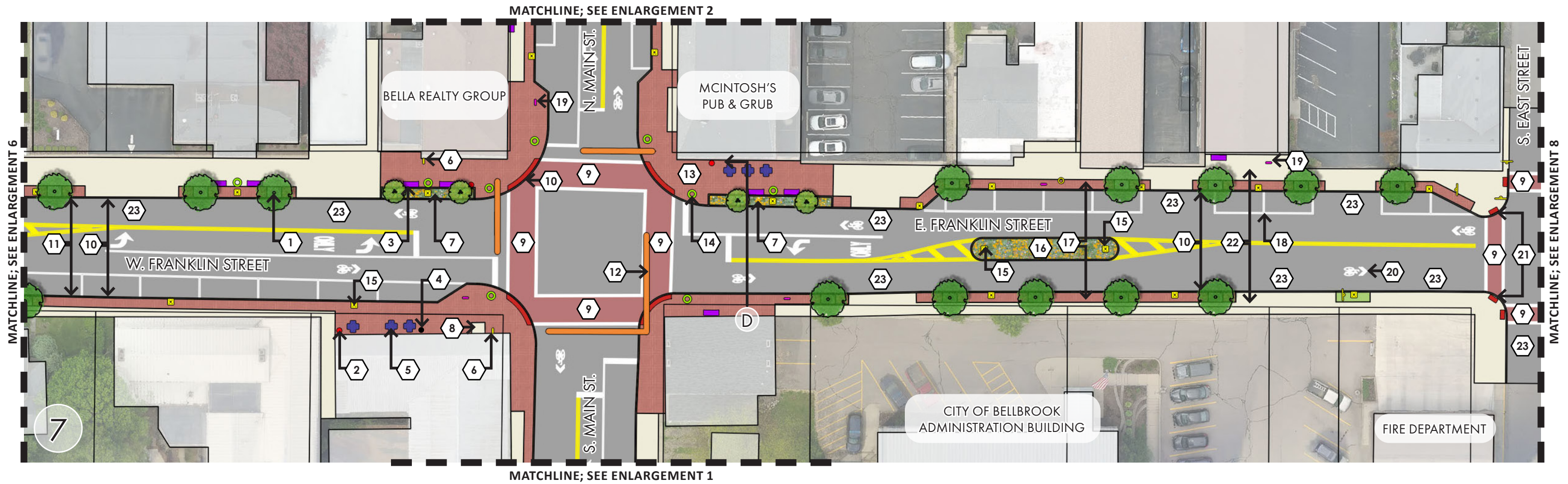
- | | | | |
|--|--|--|-----------------------------------|
| 1 PROPOSED UNDERGROUND UTILITIES | 7 PROPOSED CURB CUT | 12 PROPOSED 7' CONCRETE SIDEWALK | 17 PROPOSED RAISED PLANTED MEDIAN |
| 2 PROPOSED CURB BUMP OUT, TYP. | 8 PROPOSED SHARROW PAVEMENT MARKING, TYP. | 13 NEW PEDESTRIAN LIGHT POLE WITH BANNER AND ELECTRICAL OUTLET, TYP. | |
| 3 PROPOSED STORM SEWER | 9 PROPOSED PARKLET WITH NEW AND/OR RELOCATED BENCHES, CAFE SEATING, AND NEW PLANTERS, TYP. | 14 PROPOSED PLANT BED, TYP. | |
| 4 PROPOSED STREET TREE IN GRATE WITH ELECTRICAL OUTLET, TYP. | 10 PROPOSED RAPID FLASHING BEACON | 15 FUTURE CONNECTION TO BELLBROOK-FAIRBORN CONNECTOR BIKEWAY | |
| 5 PROPOSED 6" CONCRETE CURB | 11 PROPOSED DECORATIVE CROSSWALK | 16 PROPOSED ADA DETECTABLE WARNING PAD, TYP. | |
| 6 PROPOSED 4' DECORATIVE PAVER BANDING, TYP. | | | |

KEY MAP



Proposed Downtown Streetscape Plan

SCALE 1"=40'

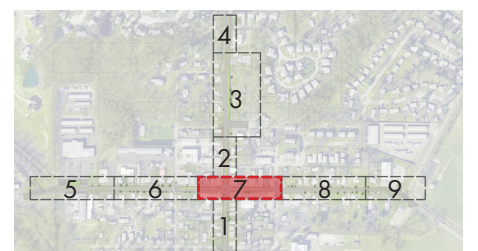


LEGEND

- CONCRETE SIDEWALK
- PAVERS
- TREE LAWN
- ADA WARNING PAD
- SIGN
- RAPID FLASHING BEACON
- NEW LIGHT POLE WITH BANNER
- A GRAPHIC SECTION CUT SEE SHEET 2.12
- NEW BENCH LOCATION
- NEW BIKE RACK LOCATION
- NEW PLANTER
- NEW TRASH CAN
- FIRE HYDRANT
- MANHOLE
- STORM DRAIN

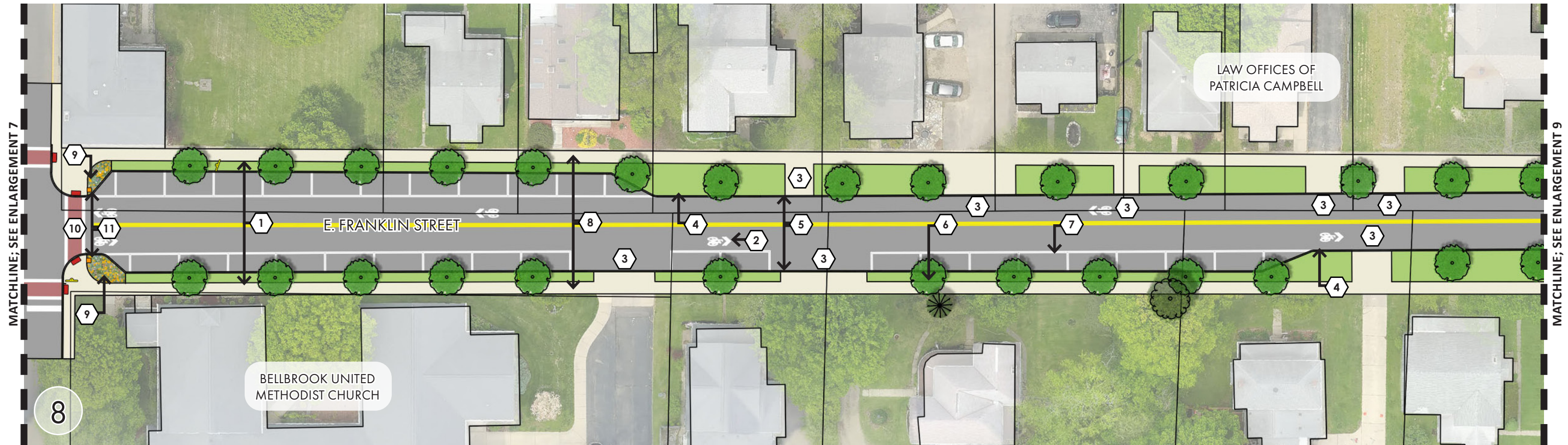
PROPOSED KEY NOTES

- | | | | |
|--|-------------------------------------|--|--|
| 1 PROPOSED STREET TREE IN GRATE WITH ELECTRICAL OUTLET, TYP. | 7 PROPOSED PLANT BED, TYP. | 14 PROPOSED PLANTER, TYP. | 20 PROPOSED SHARROW PAVEMENT MARKING, TYP. |
| 2 PROPOSED TRASH CAN, TYP. | 8 HISTORIC WAGON WHEELS | 15 NEW PEDESTRIAN LIGHT POLE WITH BANNER AND ELECTRICAL OUTLET, TYP. | 21 ADA COMPLIANT CURB RAMP AT NEW BUMP OUT KEY MAP |
| 3 PROPOSED AND/OR RELOCATED BENCH | 9 PROPOSED DECORATIVE CROSSWALK | 16 PROPOSED RAISED PLANTED MEDIAN | 22 PROPOSED CONCRETE SIDEWALK (WIDTH VAR.) |
| 4 HISTORIC WATER PUMP | 10 PROPOSED STORM SEWER | 17 PROPOSED 4' DECORATIVE PAVER BANDING, TYP. | 23 PROPOSED CURB CUT |
| 5 PROPOSED CAFE SEATING, TYP. | 11 PROPOSED 6" CONCRETE CURB | 18 PROPOSED ON-STREET PARKING STRIPING, TYP. | |
| 6 RELOCATED BUSINESS SIGN TO BUILDING | 12 PROPOSED MAST ARM TRAFFIC SIGNAL | 19 PROPOSED AND/OR RELOCATED BIKE RACK, TYP. | |
| | 13 PROPOSED PAVERS, TYP. | | |



Proposed Downtown Streetscape Plan

SCALE 1"=40'



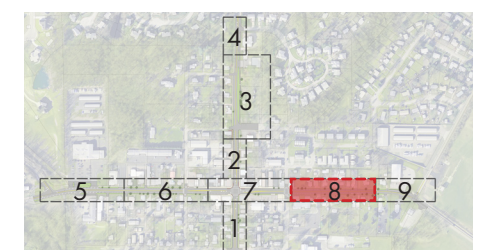
LEGEND

- | | |
|----------------------------|------------------------|
| CONCRETE SIDEWALK | NEW BENCH LOCATION |
| PAVERS | NEW BIKE RACK LOCATION |
| TREE LAWN | NEW PLANTER |
| ADA WARNING PAD | NEW TRASH CAN |
| SIGN | FIRE HYDRANT |
| RAPID FLASHING BEACON | MANHOLE |
| NEW LIGHT POLE WITH BANNER | STORM DRAIN |

PROPOSED KEY NOTES

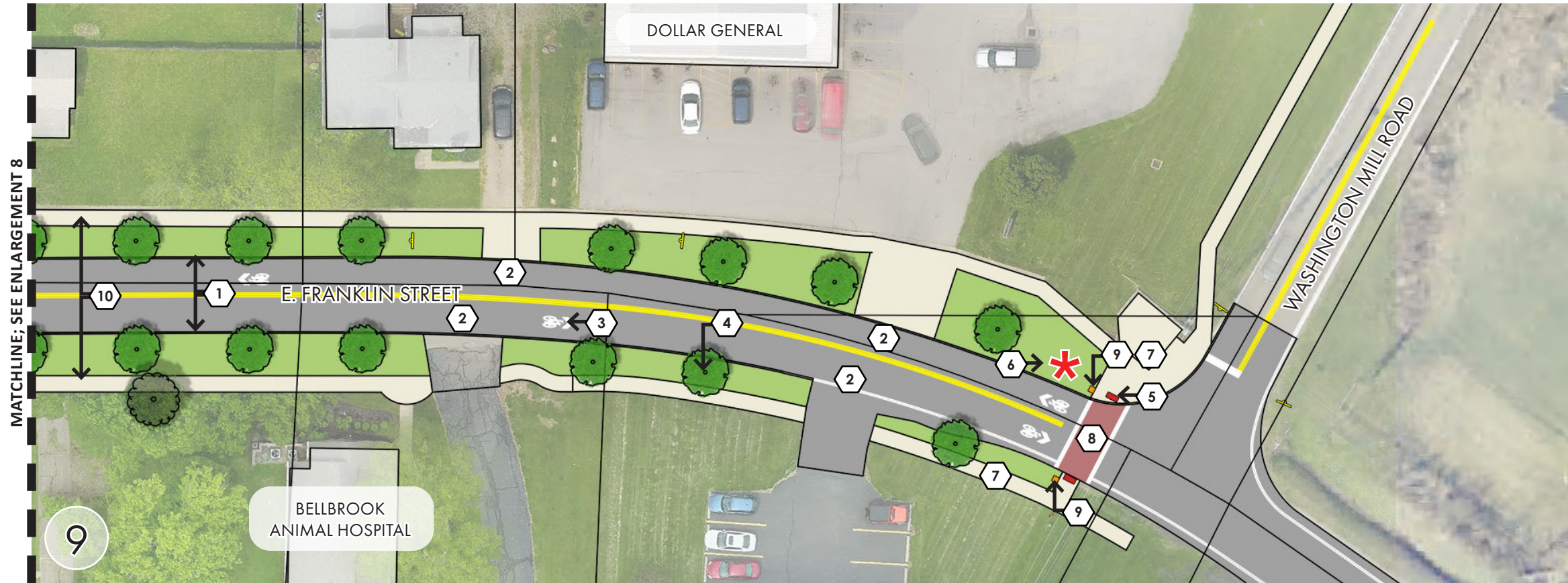
- | | |
|---|--------------------------------|
| PROPOSED STORM SEWER | PROPOSED 5' CONCRETE SIDEWALK |
| PROPOSED SHARROW PAVEMENT MARKING, TYP. | PROPOSED PLANT BED, TYP. |
| PROPOSED CURB CUT | PROPOSED DECORATIVE CROSSWALK |
| PROPOSED CURB BUMP OUT | PROPOSED RAPID FLASHING BEACON |
| PROPOSED 6" CONCRETE CURB | |
| PROPOSED STREET TREE, TYP. | |
| PROPOSED ON-STREET PARKING STRIPING, TYP. | |

KEY MAP



Proposed Downtown Streetscape Plan

↑ SCALE 1"=40'



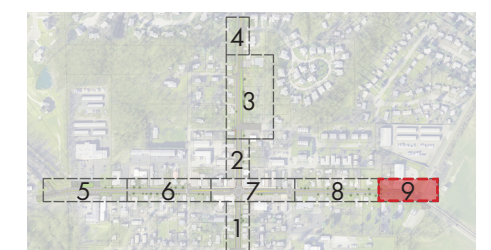
LEGEND

- | | |
|----------------------------|------------------------|
| CONCRETE SIDEWALK | NEW BENCH LOCATION |
| PAVERS | NEW BIKE RACK LOCATION |
| TREE LAWN | NEW PLANTER |
| ADA WARNING PAD | NEW TRASH CAN |
| SIGN | FIRE HYDRANT |
| RAPID FLASHING BEACON | MANHOLE |
| NEW LIGHT POLE WITH BANNER | STORM DRAIN |

PROPOSED KEY NOTES

- | | |
|---|--------------------------------|
| PROPOSED 6" CONCRETE CURB | PROPOSED DECORATIVE CROSSWALK |
| PROPOSED CURB CUT | PROPOSED RAPID FLASHING BEACON |
| PROPOSED SHARROW PAVEMENT MARKING, TYP. | PROPOSED 5' CONCRETE SIDEWALK |
| PROPOSED STREET TREE, TYP. | |
| PROPOSED ADA DETECTABLE WARNING PAD, TYP. | |
| PROPOSED GATEWAY ELEMENT | |
| PROPOSED SIDEWALK EXTENSION | |

KEY MAP



Proposed Downtown Streetscape Plan

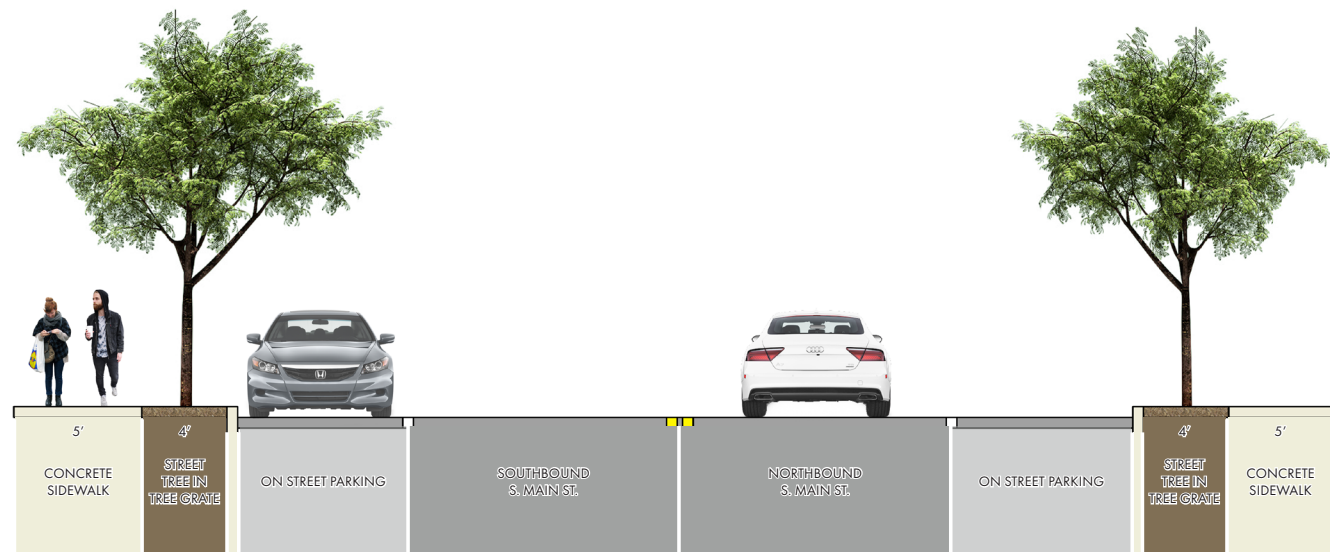
↑ SCALE 1"=40'



A SECTION LOCATION: PROPOSED MID BLOCK CROSSWALK ON W. FRANKLIN ST. NEAR WINTERS-BELLBROOK COMMUNITY LIBRARY



B SECTION LOCATION: W. FRANKLIN ST. NEAR MARATHON GAS STATION



C SECTION LOCATION: S. MAIN ST.



D SECTION LOCATION: E. FRANKLIN ST. NEAR MAIN ST INTERSECTION

Proposed Downtown Streetscape Sections



STAMPED ASPHALT CROSSWALK



PIANO KEY CROSSWALK WITH RECTANGULAR RAPID FLASHING BEACONS



NARROWED ROAD FOR TRAFFIC CALMING



DECORATIVE CONCRETE PAVERS



DECORATIVE BRICK PAVERS



SHARROW BIKE LANE MARKINGS



BUMP OUTS PROVIDE OPPORTUNITIES FOR OUTDOOR SEATING



LANDSCAPED TREE PITS



BUMP OUTS CREATE PROTECTED ON-STREET PARKING

Hardscape Palette



FORMS AND SURFACES URBAN RENAISSANCE LITTER AND RECYCLING RECEPTACLE WITH CUSTOM BRANDING



HISTORICAL BUILDINGS TO HAVE NEW COMMEMORATIVE PLAQUES



EXISTING BIKE RACK STYLE TO REMAIN



STREET TREE IN TREE GRATE WITH ELECTRICAL OUTLET



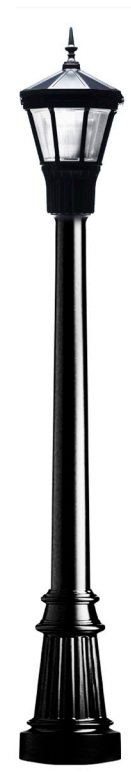
LANDSCAPE FORMS SORELLA PLANTERS WITH CUSTOM BRANDING



EXISTING BENCH STYLE TO REMAIN



PROPOSED WAYFINDING SIGNAGE



PEDESTRIAN LIGHT POLE WITH BANNER AND ELECTRICAL OUTLET

Furnishings Palette



BANNER POLE



CHANGEABLE BANNER



MONUMENT SIGN



GATEWAY ARCH



PILLAR MONUMENT SIGN

Gateway Element Ideas

DECIDUOUS TREES



IVORY SILK TREE LILAC - PROPOSED UNDER OVERHEAD UTILITIES



ARMSTRONG MAPLE



GREEN VASE ZELKOVA



AUTUMN GOLD GINKGO

ORNAMENTAL GRASSES



DWARF FOUNTAIN GRASS



LITTLE BLUESTEM GRASS

PERENNIALS



BLACK EYED SUSAN



CATMINT



VARIEGATED LIRIOPE

Landscape Palette

Cost Estimate

Site Furnishings

| | <u>Qty</u> | <u>Unit</u> | <u>Unit Cost</u> | <u>Amount</u> |
|--|------------|-------------|----------------------|---------------|
| Benches | 7 | EA | \$3,000.00 | \$21,000.00 |
| Planters | 19 | EA | \$3,000.00 | \$57,000.00 |
| Tables & Chairs | 7 | EA | \$3,000.00 | \$21,000.00 |
| Trash Receptacles | 19 | EA | \$3,000.00 | \$60,000.00 |
| Contingency, Escalation, Prevailing Wage - 15% | 1 | ALLOW | - | \$22,500.00 |
| Soft Cost - 4% | 1 | ALLOW | - | \$6,900.00 |
| Site Furnishings Subtotal: | | | ±\$180,000.00 | |

Utilities

| | <u>Qty</u> | <u>Unit</u> | <u>Unit Cost</u> | <u>Amount</u> |
|--|------------|-------------|------------------------|----------------|
| Underground Electrical / Fiber / Telecom | 1 | ALLOW | \$1,700,000.00 | \$1,700,000.00 |
| Storm Sewer & Curb Underdrains | 1 | LS | \$400,000.00 | \$400,000.00 |
| Light Pole/ Banner Poles / Banner Arms | 59 | EA | \$7,000.00 | \$413,000.00 |
| Contingency, Escalation, Prevailing Wage - 15% | 1 | ALLOW | - | \$373,200.00 |
| Soft Cost - 8% | 1 | ALLOW | - | \$228,896.00 |
| Utilities Subtotal: | | | ±\$3,090,000.00 | |

Roadway Improvements

| | <u>Qty</u> | <u>Unit</u> | <u>Unit Cost</u> | <u>Amount</u> |
|--|------------|-------------|------------------------|---------------|
| Demolition of Asphalt Bump Out Areas | 2,665 | SY | \$6.00 | \$15,990.00 |
| Demolition of Concrete Curb | 3,190 | LF | \$1.25 | \$3,987.50 |
| Mill & Overlay | 17,333 | SY | \$18.00 | \$311,994.00 |
| Re-striping & Pavement Markings | 1 | LS | \$20,000.00 | \$20,000.000 |
| New Concrete Curb & Gutter (6") | 8,555 | LF | \$26.00 | \$222,430.00 |
| Rapid Flashing Beacon (per Crosswalk) | 6 | EA | \$20,000.00 | \$120,000.00 |
| New Signal Poles Mast Arm (Franklin & Main) | 1 | LS | \$230,000.00 | \$230,000.00 |
| Decorative Crosswalks (Stamped Thermoplastic) | 5,125 | SF | \$25.00 | \$128,125.00 |
| Contingency, Escalation, Prevailing Wage - 15% | 1 | ALLOW | - | \$157,890.00 |
| Soft Cost - 8% | 1 | ALLOW | - | \$96,840.00 |
| Roadway Improvements Subtotal: | | | ±\$1,310,000.00 | |

Streetscape Improvements

| | <u>Qty</u> | <u>Unit</u> | <u>Unit Cost</u> | <u>Amount</u> |
|--|------------|-------------|------------------------|---------------|
| Demo Existing Sidewalks / Driveway Aprons | 41,510 | SF | \$1.50 | \$62,265.00 |
| Concrete Sidewalks / Driveway Aprons | 44,560 | SF | \$8.50 | \$378,760.00 |
| Brick Paving | 14,740 | SF | \$20.00 | \$294,800.00 |
| Curb Ramps | 44 | EA | \$500.00 | \$22,000.00 |
| Tree Grates | 70 | EA | \$5,000.00 | \$350,000.00 |
| Contingency, Escalation, Prevailing Wage - 15% | 1 | ALLOW | - | \$166,185.00 |
| Soft Cost - 8% | 1 | ALLOW | - | \$101,928.00 |
| Streetscape Improvements Subtotal: | | | ±\$1,375,000.00 | |

Landscape Improvements

| | <u>Qty</u> | <u>Unit</u> | <u>Unit Cost</u> | <u>Amount</u> |
|--|------------|-------------|----------------------|---------------|
| Deciduous Street Tree (3" Cal.) | 137 | EA | \$700.00 | \$95,900.00 |
| Ornamental Tree (2" Cal.) | 23 | EA | \$500.00 | \$11,500.00 |
| Plant Bed (Topsoil, Mulch, Plant Material) | 5,070 | SF | \$10.00 | \$50,700.00 |
| Contingency, Escalation, Prevailing Wage - 15% | 1 | ALLOW | - | \$23,715.00 |
| Soft Cost - 8% | 1 | ALLOW | - | \$14,552.00 |
| Landscape Improvements Subtotal: | | | ±\$197,000.00 | |

Gateway Signage

| | <u>Qty</u> | <u>Unit</u> | <u>Unit Cost</u> | <u>Amount</u> |
|--|------------|-------------|----------------------|---------------|
| Gateway Monument Signage | 2 | ALLOW | \$60,000.00 | \$120,000.00 |
| Contingency, Escalation, Prevailing Wage - 15% | 1 | ALLOW | - | \$18,000.00 |
| Soft Cost - 8% | 1 | ALLOW | - | \$11,040.00 |
| Gateway Signage Subtotal: | | | ±\$150,000.00 | |

Bellbrook Park Improvements

| | <u>Qty</u> | <u>Unit</u> | <u>Unit Cost</u> | <u>Amount</u> |
|--|------------|-------------|---------------------|---------------|
| Entrance Plaza & Branding Element | 1 | LS | \$35,000.00 | \$35,000.00 |
| Benches | 2 | EA | \$3,000.00 | \$6,000.00 |
| Planters | 6 | EA | \$3,000.00 | \$18,500.00 |
| ADA Accessible Pathway | 2,000 | SF | \$8.00 | \$16,000.00 |
| Contingency, Escalation, Prevailing Wage - 15% | 1 | ALLOW | - | \$11,250.00 |
| Soft Cost - 8% | 1 | ALLOW | - | \$6,904.00 |
| Bellbrook Park Improvements Subtotal: | | | ±\$95,000.00 | |

Total: ±\$6,390,000.00

Grant Funding Sources

Funding options for this project will ultimately depend on the overall scope of work and the City's preferred timeline for the project. Certain funding sources are geared toward pedestrian and beautification projects, while others will not fund that work as a standalone project or will outright exclude those items.

The project size would lend itself more toward federal funding sources with more available dollars, such as Surface Transportation Program (STP), Transportation Alternatives (TA), and Congestion Mitigation and Air Quality (CMAQ). These programs offer substantial dollars for grant funding, but they also require a complicated project development process, stretching the timeline prior to construction out 4-5 years. The Kleingers Group has prepared applications and successfully received funding from each one of these programs in the past.

Similarly, ODOT's Safe Routes to School (SRTS) program is another possible funding source which would stretch project schedule in a similar fashion. The City/School District would have to update the existing School Travel Plan to include the project area as a site of interest. The current plan does not identify the downtown area as an area of interest for safety measures related to any school in the district. The City would need to work this area into the plan in order to advance any discussions related to this funding source.

Ohio Public Works Commission (OPWC) is a potential funding source that could be leveraged for this project. These funds are more readily available and allow the project to move forward on a faster schedule. However, standalone pedestrian or bikeway projects are not eligible for OPWC funds. In order to take advantage of these funds, the project scope would have to be expanded to an extent that includes significant eligible costs, such as roadway and storm sewer improvements. The Kleingers Group has also successfully prepared OPWC applications for numerous communities and projects in the past.

ODOT's Highway Safety Improvement Program (HSIP) is another funding source which could be leveraged for this project. Only certain types of roadways are eligible for these funds, with West Franklin being the only portion of this project that would meet the criteria. For pedestrian safety improvements, up to \$2 million dollars can awarded for a project, with a 10% local match required. Funding guidance suggests that safety measures be "widely implemented". This could be met given the overall size of the project, but pedestrian safety measures should be proposed along the corridor in order to demonstrate this. For the upcoming funding cycle, application for this program will be due on January 31, 2022.

Additional programs with less available dollars, such as Greene County CDBG could also be explored as matching dollars to reduce local funds needed. The recommended funding approach for this project would also be tied to local dollars available and the anticipated scope of the project. Assuming the scope includes roadway and storm improvements, and thus expanding eligibility for more funding sources, a suggestion would be to acquire the largest piece of funding (80%-90%) from a federal transportation program such as STP. Once that funding is secured, smaller or more specialized programs (OPWC, CDBG) could then be leveraged to cover the remaining local share. Again, accessing federal funding dollars would stretch the project timeline, but The Kleingers Group has taken this approach in past project planning, in some cases leveraging multiple sources to cover the entire project cost.



Public Input Summary

A public open house was held on September 29th, 2021 from 6:00pm - 8:00pm at 15 W Franklin St. to gather feedback from the Bellbrook community; below are the comments received organized by category.

Transportation / Roadway

- Concerns with the median in front of gas station. Will it be difficult for large vehicles to enter/exit?
- Will bump-outs at Franklin and Main make turning difficult for large vehicles (already tight)?
- Not a fan of curb bump-outs.
- Less curb cuts to add parking.
- Snow would cover median in the winter
- Curb bump-outs a problem with snow removal–Lofino Plaza.

Connectivity

- Love most of this, but the blinking signs at the crosswalk do not fit the historical motif.
- Agree with a crosswalk on Main to Bellbrook Park.
- Push button integrated at crosswalks.
- Crosswalk from Library to Dairy Shed is a great idea.

Utilities

- Would like all utilities to run underground.

Site Furnishings & Gateway Sign

- Love the Bellbrook arched gate signs.
- Keep existing benches that are donated by organizations within plan.
- Keep signs–lions, garden club, optimist club, and scouts.
- Business signs with historic look–hanging sign on storefronts.
- Business signs should be uniform in size.
- Gateways should have more historic feel.
- Outdoor speakers throughout walking district–good quality, low volume music to be on during community events.
- In median, maybe only one banner pole.
- Victorian lamp post with electrical outlets for holiday or other lighted fixtures–town clock/post-like lamp posts.
- Flowers–pots or lamp post fixtures.
- New old village district signs on all four streets heading into town.

Design of the Downtown District

- Love the red brick pavers.
- Use stamped concrete on sidewalk.
- Cobblestone, brick, or stone sidewalks throughout village district–start with town center depending on money.
- Small ornamental trees.
- Green space with trees and plants.

Summary

- Providing other amenities to complement the streetscape could potentially draw people to Bellbrook's downtown district.
- The public agrees with adding upgraded crosswalks and mid-block crossings as safety measures. Some did not prefer the rapid flashing beacons due to their visual obtrusiveness.
- Bump-outs and medians are a concern in terms of maintenance and for turning radius' for large vehicles. Review turning radii for bump outs and entrance into the gas station.
- Underground utilities would be a beneficial improvement to the aesthetics of the streetscape.
- The public likes the idea of improving site furnishings. Existing locations of benches and bike racks should be studied for better usage. Improved pedestrian scale lighting, signage, banner poles, etc. were well received.
- Gateway signage options received mixed reviews. Most preferred the gateway arch and/or pylon sign options.
- Responses to the proposed landscaping improvements were mainly positive. Consistent street tree placement and species was preferred. Increasing use of flowering perennials was also well received.
- Outdoor dining was preferred at the intersection of Franklin and Main.
- Most attendees preferred the stamped asphalt crosswalks and brick pavers within the streetscape design.
- Attendees generally agree with the proposed beautification of the downtown district streetscape, but prefer that a historic feel is maintained.



THE KLEINGERS GROUP PRESENTING BOARDS TO THE COMMUNITY

Public Input Summary (Social Media)

After the public open house held on September 29th, 2021 the Bellbrook community continued to share feedback through social media on the Bellbrook Sugarcreek Community Billboard Facebook page.

Transportation / Roadway

- Not completely sold on the curb bump-outs to slow down traffic. Would like to see better examples of where this has been successfully implemented.
- Accessible curb cuts at the intersection are great—yes, we are prejudiced about this one.
- A crosswalk at the Dairy Shed is truly needed, but I am afraid there could be casualties before distracted drivers realize it's there!
- No median or bump-outs are needed. Main St and Franklin St are not wide enough to accommodate them.
- Curb bump-outs were attempted years ago. With Tom's Mulch and Ernst going through town, it's not a viable option unless you like bump-outs with tire marks on them.
- Won't a curb bump-out take up more parking spots?
- Curb bump-outs did not work the first time. The cost of installing the bump-outs and later removing them was pretty high. It made it very difficult for tractor trailers and school buses to make the turn in the center of town.
- No curb bump-outs!
- Cutting trees and eliminating residents parking? No!
- Speed bumps on S Main!
- Bump outs at the corners - the idea that they protect parking is iffy at best. I don't ever recall a parked car being hit. It's already difficult to turn onto the narrow streets and the bump outs make it even harder. I have witnessed so many larger vehicles run over the bump outs trying to turn - primarily because a lot of them try to cut the corner too short, but also because the turns are so tight. Also, it makes parking more difficult.
- No median planters or installations. Definitely no plantings there. They are a maintenance nightmare! The painted lines on the pavement are sufficient and much easier to maintain. I would be very surprised if having them there would slow anyone down if they are intent of speeding. The only mechanisms I think that slows traffic is a reputation for ticketing speeding or roundabouts and we certainly do not have the space for those. They would have to be so tight that the school buses would never be able to negotiate them!

Connectivity

- Need a sidewalk that goes from the high school all the way into downtown be made safer. I am always terrified to walk it. The traffic moves so fast, and there is nothing separating the pedestrians from those cars. I think a lower speed limit, plus a guard rail has been needed for years. Especially since so many children walk that route.
- Additional walkways connecting the downtown to adjacent parks would be great.

Utilities

- Loved the idea of burying the power lines, but the costs are probably (most likely) prohibitive.

Site Furnishings & Gateway Sign

- The gateways at each end of town would be great. I would like to see them with a more historical context to match the community feel.
- No more planters are needed.
- Love the more traditional ideas, historical marker, improved trash receptacles, street lighting, seasonal flags, improved landscaping.
- Love the gateway/arch idea.
- Has anyone suggested standardizing the signage? When you drive down the street all you see is a bunch of business signs. All sizes and shapes everywhere. It looks trashy.
- All ideas sound great and downtown could definitely use a facelift.
- I do hope that when we do upgrade our lighting that we adhere to the Dark Sky guidelines to avoid lighting up the night so it looks like daytime. So many negatives attached to that model. I have information on that program and had hoped to introduce it before COVID distracted us. Maybe it's time to look at it now?
- And finally, the planters - it was hard to tell if they were recommending replacing what we have or just showing examples of city planters - what they showed was very sleek, modern looking ones, not in keeping with our historic look. The city paid for the current planters, but the garden club selected them and maintains them, except for watering, which the service guys do.

Summary

- While some individuals are for implementing curb bump-outs, other feel they are not effective and reduce parking spaces for residents living downtown.
- Availability of on-street parking is a concern.
- Reducing vehicular speed along Franklin & Main is desired.
- The gateway sign and improved site furnishings ideas were well received.